

COUNTY OF SANTA BARBARA PUBLIC WORKS DEPARTMENT
TRANSPORTATION DIVISION

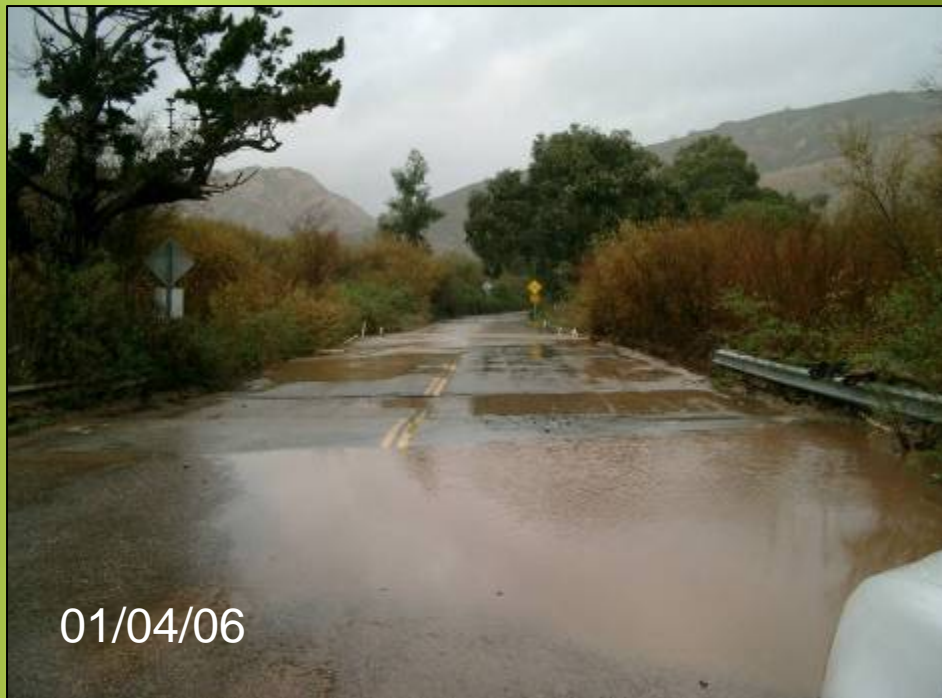


Gaviota Bridge Replacement

BOARD OF SUPERVISORS APPEAL HEARING - March 14, 2006



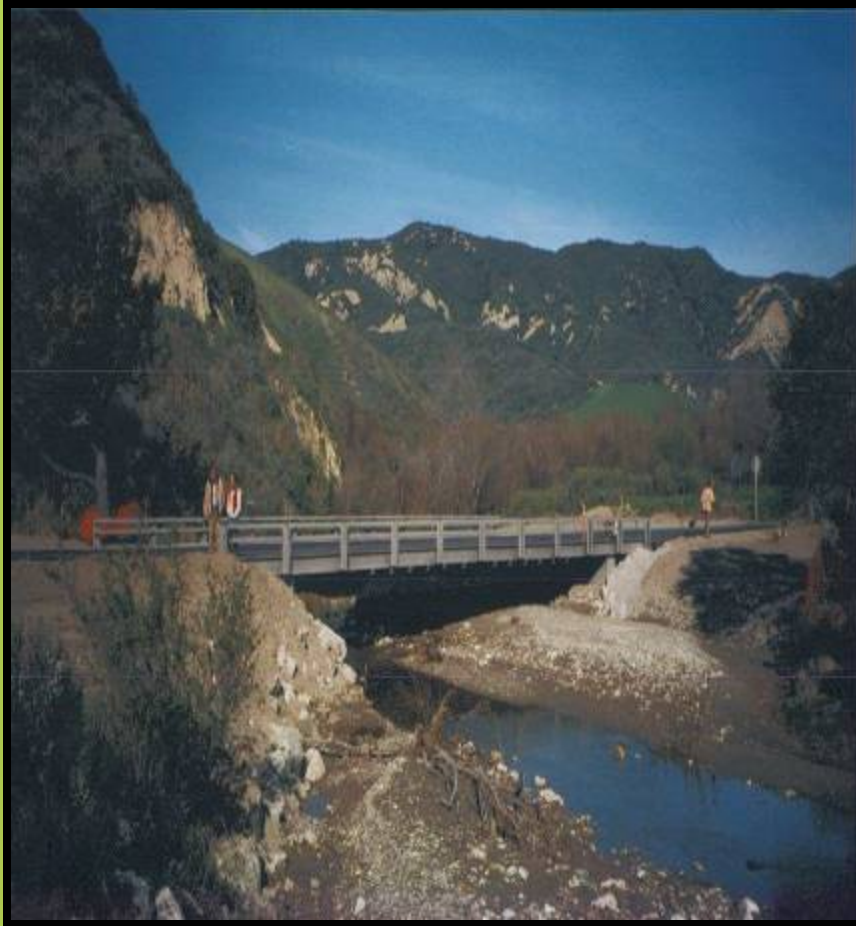
Public Works Project Team



- Dace Morgan
Engineering Manager
- Scott McGolpin,
Dept. Dir. Transportation
- URS Corporation
Environmental Consultant
- Quincy Engineering
Design Consultant



Project History



- Provides the only access to Gaviota State Park (200,000 annual visitors) and Hollister Ranch (500 residents)
- 80 foot long railroad car at existing roadway grade constructed in 1997
- 6-8 foot clearance under bridge when built



History of Flooding



- The Bridge, Roadway and the State Park are within the 10 year flood plain
- Sediment has accumulated under the bridge
- Existing roadway is routinely flooded during winter rains stranding residents for hours or days
- Heavily damaged in 1998 El Nino storms



Funding History

- Bridge damaged in 1998 federally declared emergency
- Funding secured for bridge repair and debris removal
- County unable to secure permits to remove sediment from under bridge to restore capacity
- 2001 FEMA approved funding to design and complete environmental clearance for full bridge replacement to convey 100-year event for health and safety reasons
- 2005 (following NEPA clearance) FEMA approved funding for construction



Inclusive Design Process



- Project Development Team (PDT) formed in 2002 to design cost effective and environmentally sound project
 - Consultants, FEMA, OES, State Parks, Hollister Ranch
- Meeting held with Resource and Permitting Agencies in late 2002
 - ACOE, NOAA Fisheries, DFG, USFWS, CCC
 - Overwhelming support for proposed project due to environmental benefits



Project Benefits

- Permanent and safe solution to routine flooding of bridge and roadway into Gaviota State Park and Hollister Ranch
- Ensures year round access to recreational opportunities at Gaviota State Park and to Hollister Ranch's lifeline
- Provides an all weather surface to provide improved Health and Welfare for the Traveling Public
- No cost to Local Taxpayers



Alternatives Considered

- **Causeway alternative**

- Viaduct Bridge for the entire length of the roadway
- Additional costs approximately \$2 million

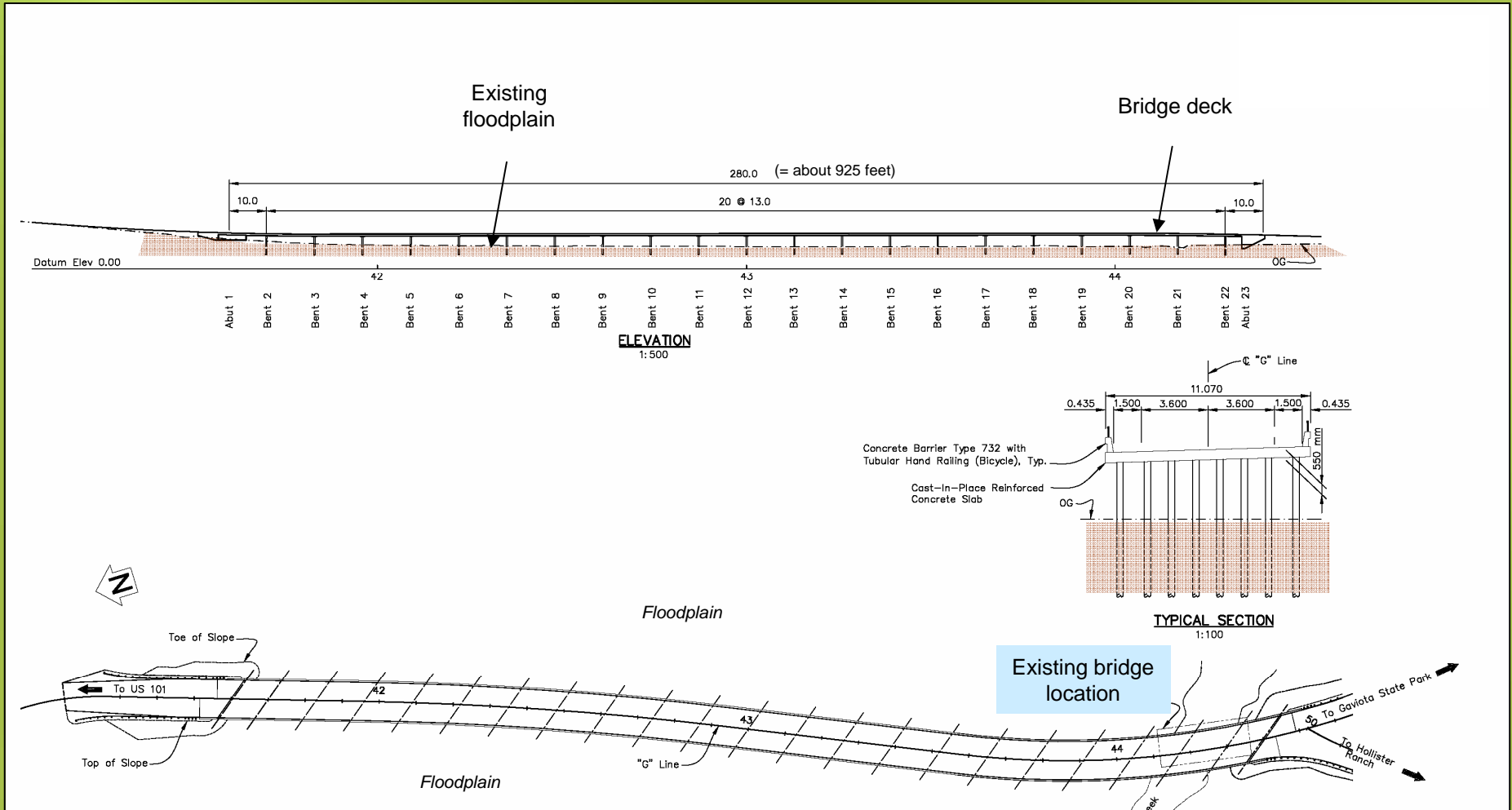
- **Alternative bridge site**

- Relocate the creek crossing and the entrance road to Road 28
- Significant cost to construct a full interchange with US 101





Causeway Alternative





Alternative Bridge Location



Alternatives are Infeasible

- Causeway alternative is considered infeasible due to cost, an additional \$2 million would be required for this project
- Alternative bridge locations are not environmentally superior, would require acquisition of State Parks land, significant stabilization to Road 28, and would not be allowed by Caltrans without a new full interchange to Highway 101



The Proposed Project

- County has recently requested an additional time extension from FEMA due to the Environmental concerns that have been raised recently
- Most Cost Effective Design to meet the design criteria
- Addresses current hazardous condition during times of flooding
- Competed Nationally for the \$2.2 million in Hazard Mitigation Funding
- The proposed project is ALL that FEMA and OES will pay for
- This is the right project and the only fiscally viable project



Threat to Project Funding

- If we do not receive the requested time extension, construction will have to commence in summer 2006 in order preserve our funding
- Additional costs for an alternative project are not eligible for additional funding
- Any change to design would require new cost benefit analysis, new environmental review and more time
- Such a request would jeopardize entire FEMA funding package – the \$2.2 million in HMP, the \$800,000 for the bridge and possibly all costs to date



Status Quo





Thank You



01/04/06