

# SANTA BARBARA COUNTY PLANNING COMMISSION

## Staff Report for the

### Mission Park to Mission Canyon Multimodal Improvements Plan

**Hearing Date:** December 10, 2014

**Staff Report Date:** November 20, 2014

**Case No.:** Not applicable

**Environmental Document:** CEQA Exemption  
§15262 and 15061(b)(3)

**Deputy Director:** Matt Schneider

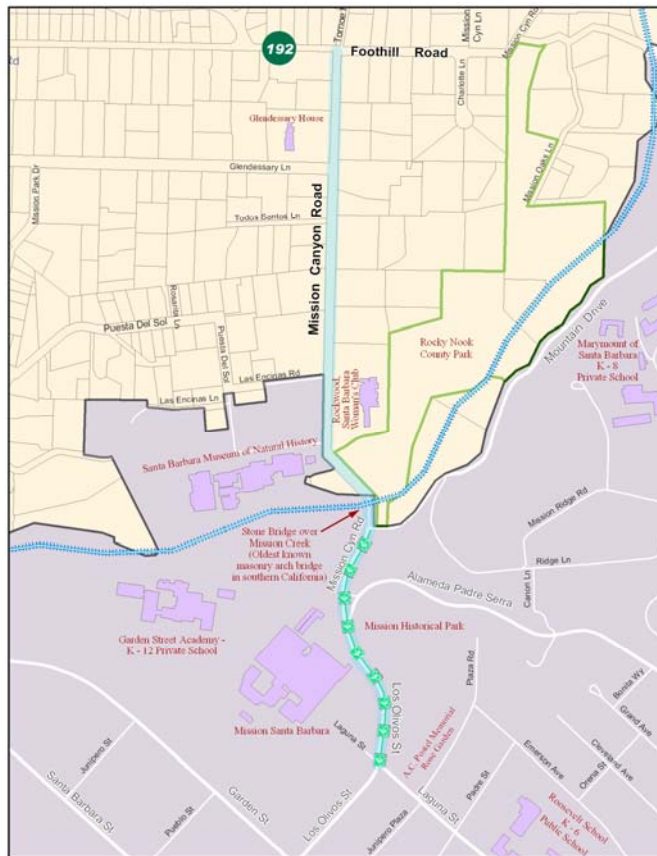
**Division:** Long Range Planning

**Supervising Planner:** Allen Bell

**Supervising Planner Phone #:** 568-2056

**Staff Contact:** Julie Harris

**Planner's Phone #:** 568-3518



## 1.0 REQUEST

Hearing of the County Planning Commission on the request of the County Planning and Development Department to consider recommending that the Board of Supervisors receive and refer the Mission Park to Mission Canyon Multimodal Improvements Plan to County Public Works to include those improvements located within the County's jurisdiction in the County 2015-2020 Capital Improvement Program; adopt resolution directing County staff to work collaboratively with City of Santa Barbara staff to pursue funding opportunities and to implement the plan; and determine the project is exempt from the provisions of the California Environmental Quality Act (CEQA) pursuant to State CEQA Guidelines Sections 15262 and 15061(b)(3). The improvements involve the public road between the intersection of Laguna Street and East Los Olivos Street, northward to the intersection of Mission

Canyon Road and Foothill Road, located in the City of Santa Barbara and the unincorporated area of Santa Barbara County (Mission Canyon Plan Area), First and Second Supervisorial Districts. Therefore, the plan includes concepts for improvements in both the City's jurisdiction and the County's jurisdiction.

## **2.0 RECOMMENDATION AND PROCEDURES**

Your Commission's motion should include the following:

1. Recommend that the Board of Supervisors receive and refer the Mission Park to Mission Canyon Multimodal Improvements Plan, included as Attachment A, to County Public Works to include those improvements located within the County's jurisdiction in the County 2015-2020 Capital Improvement Program.
2. Recommend that the Board of Supervisors adopt a resolution, included as Attachment I, directing County staff to work collaboratively with City of Santa Barbara staff to pursue funding opportunities to implement the Mission Park to Mission Canyon Multimodal Improvements Plan.
3. Recommend that the Board of Supervisors determine both actions to be exempt from CEQA pursuant to CEQA Guidelines Sections 15262 and 15061(b)(3), included as Attachment B.

## **3.0 JURISDICTION**

The Mission Park to Mission Canyon Multimodal Improvements Plan (plan) is being considered by the County Planning Commission for review, comment, and recommendation prior to consideration by the Board of Supervisors because the plan pertains to transportation planning issues. Specifically, Section 2-25.2(b)(3) of the Santa Barbara County Code states:

*... the following shall remain within the jurisdiction of the county planning commission ... consideration and recommendations or decisions on ... matters involving countywide transportation ...*

Neither the California State Government Code nor the County Code requires that the Planning Commission or the Board of Supervisors take action on this matter. However, referral of the Mission Park to Mission Canyon Multimodal Improvements Plan to the Public Works Department for inclusion in the County's Capital Improvement Program would strengthen funding options for future implementation of the plan.

## **4.0 PROJECT INFORMATION**

### **4.1 Introduction**

This project is a grant-funded partnership between the County of Santa Barbara (County) and the City of Santa Barbara (City) to prepare concept level plans for pedestrian, bicycle, and vehicle circulation improvements in this historic and highly scenic corridor. The plan area extends from

the intersection of Laguna Street and East Los Olivos Street at Mission Santa Barbara (in the City) to the intersection of Mission Canyon Road and Foothill Road (in the County). Thus, the hearing on December 10, 2014, will be a joint hearing of the County and City Planning Commissions.

No continuous pedestrian or bicycle paths, lanes, or routes exist through this busy corridor. Although a few facilities exist at various points, they do not comply with the American with Disabilities Act (ADA). The lack of continuous facilities makes walking and biking in the area difficult and hazardous for residents and visitors.

In 2012, the California Department of Transportation (Caltrans) awarded a Community-Based Transportation Planning grant (\$88,911) for the Mission Park to Mission Canyon Multimodal Improvements Plan to the Santa Barbara County Planning and Development Department and the City of Santa Barbara Public Works. Additional funds included a local cash match (\$6,669; County) and local in-kind match (\$3,000; Santa Barbara Woman's Club and Santa Barbara Museum of Natural History).

The project is limited to concept level plans. Detailed engineering, traffic studies, and environmental review were not included in the scope of work of the Caltrans grant. These tasks will be undertaken as part of a subsequent implementation phase. Because the project only provides concept plans for possible future action, the Board of Supervisors is not required to adopt or fund it at this time. The plan does not commit the County to any final course of action, nor does it commit any funds to implement the concept plans. Thus, the plan is exempt from CEQA pursuant to Section 15262 of the CEQA Guidelines (Attachment B). The resolution simply directs County staff to continue working collaboratively with City staff to pursue funding to implement the plan. As a result, the resolution is exempt from CEQA pursuant to Section 15061(b)(3) of the CEQA Guidelines because it would not have a significant effect on the environment. Public works departments will conduct environmental review when necessary as proposed improvements are funded and implemented.

## **4.2 Policy Direction**

The plan is the first step to implement policies and action items of the County's recently adopted Mission Canyon Community Plan (April 2014), in particular Action CIRC-MC-2.2, Policy CIRC-MC-3, Action CIRC-MC-3.1, and Policy VIS-MC-3.2. Numerous policies and implementation strategies from the City Circulation Element also support making and/or improving walking connections such as those proposed by the plan. Most relevant is Policy 5.1.2, which includes, "[i]mproving pedestrian access in and around the Mission ..." In addition, the plan area is specifically identified in the City's Pedestrian Master Plan as a sidewalk infill project, Policy 1.1, Policy 1.2, and Policy 1.3. Attachment C lists the complete text of the relevant County and City policies and actions.

## **4.3 Public Outreach and Participation**

Public participation and support were crucial objectives of this planning effort. Public outreach included creating a webpage on the County's website, developing a self-guided tour booklet to the corridor, and producing a video for airing on the City TV channel and the City's website. Public

outreach also included two public workshops. At the first workshop, participants provided feedback on what works well in the corridor and what needs to be fixed. County and City staff used the public feedback to identify community preferences and draft concept plans. The concept plans were presented at a second workshop. The results of the workshop confirmed overwhelming community support (89% in favor) for the concept plans included in the plan, which focused on a continuous west side pedestrian pathway.

In addition, the City of Santa Barbara Public Works Department, working with County Public Works, conducted a study of portions of the corridor within the City's jurisdiction. The study focused on the physical constraints within the corridor and design concepts that could overcome given constraints. The design concepts arising from this study influenced the development of the continuous west side pedestrian pathway. The City prepared a report that provides the technical background of this work effort (Attachment J).

Although the concept plans were derived from the community outreach process and County/City public workshops, special recognition should be given to the *Mission Heritage Trail Association* (previously *Concerned Citizens for Safe Passage Working Group, Safe Passage*), which is a diverse local group consisting of institutional, civic, and neighborhood associations, and private property stakeholders who are interested in improving pedestrian, bicyclist, and vehicle circulation in the area. Volunteers with this group conducted their own community presentations, assisted at County/City workshops, and, working with County and City engineers, provided artistic design renderings that illustrate the concept plans included in the plan.

Following the public workshops, staff presented the concept plans to the County South Board of Architectural Review (SBAR), County Historic Landmarks Advisory Commission (HLAC), County Park Commission, City Historic Landmarks Commission (HLC), and City Transportation and Circulation Committee (TCC). Minutes for each meeting are attached to this staff report (Attachments D through H).

#### **4.4 Mission Park to Mission Canyon Multimodal Improvements Plan**

The plan describes the existing setting and public outreach process, and presents the concept plans for multimodal improvements and a west side pedestrian path along the plan corridor (Attachment A). The plan also includes a brief overview of basic cost estimates, funding options, and phasing recommendations. Although the plan also presents several "long term concepts," these were not included in the final selection of concept plans because they would not be achievable in the short term, and involve complex planning issues and divergent public opinions.

Based on community input and consensus, the County and City developed concept plans that include the following key components.

- Continuous pedestrian path on the west side of the corridor. The path must comply with the ADA and be consistent with the historical setting.
- Landscape buffers between path and roadway, where feasible.
- New detached pedestrian bridge parallel and west of the existing stone bridge over Mission Creek.



- Bike lanes extending north to Puerta del Sol transitioning into bike routes in the County's jurisdiction where the road narrows.
- Relocation of several crosswalks.
- Left-turn pocket from Mission Canyon Road to Las Encinas Lane.

To accomplish these improvements, two stone walls would need to be relocated near the bridge (City), portions of the roadway would be realigned to the east between the bridge and Las Encinas Road (City and County), the road would be restriped in some areas (City and County), and the existing asphalt crown, built up over years of roadway repaving, would be ground down (County). A concern has been raised regarding the historic stone walls that would need to be relocated. The City is currently preparing an historic resources assessment to address this concern.

#### **4.5 Implementation**

Additional steps will be required to implement the concept plans. Following the Planning Commissions' recommendations, County and City staffs will request that the Board of Supervisors and City Council, respectively, receive and refer the plan to the public works departments for incorporation into their respective capital improvement programs. Implementation of specific improvements will occur balanced with other transportation planning priorities. Staff will also recommend that the Board of Supervisors and the City Council adopt separate resolutions directing their staffs to pursue funding opportunities to implement the plan (Attachment I). County staff will take the lead on improvements within the County jurisdiction and City staff will take the lead on improvements within the City jurisdiction. Both staffs will continue to collaborate pursuant to the direction in the resolutions.

Implementation will require several steps to move the concept plans forward to the point of construction. These steps include, but are not limited to:

- Formal survey of the road right-of-way, where required.
- Preliminary design engineering.
- Environmental review pursuant to CEQA.
- Review by County and City committees, where needed.
- Permits and/or rights-of-way, where needed.
- Final design engineering.

Implementation of the continuous west side pedestrian path and bicycle and vehicle circulation improvements would occur as funds become available. However, certain components of the plan that would be easier and less expensive to construct could potentially move forward sooner than those that require complex design engineering. For example, in the County, roadway pavement work, restriping, and the addition of a left-turn pocket at Las Encinas Road would improve circulation and increase site distance and safety for vehicles entering Mission Canyon Road. These elements would not require significant additional engineering and would not affect existing structures. Implementation of other components, such as the new pedestrian bridge, would take longer to complete as they would require bridge design work, environmental review, and other tasks listed above.

## **ATTACHMENTS**

- A. Mission Park to Mission Canyon Multimodal Improvements Plan, dated December 2014 (copy available at [http://longrange.sbcountyplanning.org/planareas/mission\\_canyon/mc\\_multimodal.php](http://longrange.sbcountyplanning.org/planareas/mission_canyon/mc_multimodal.php))
- B. Notice of Exemption
- C. County and City Policies and Actions
- D. SBAR Minutes
- E. County Historic Landmarks Advisory Commission Minutes
- F. County Park Commission Minutes
- G. City Historic Landmarks Commission Minutes
- H. City Transportation and Circulation Committee Minutes
- I. Draft Board of Supervisors Resolution
- J. City Conceptual Design Study, dated December 2014 (copy available at [http://longrange.sbcountyplanning.org/planareas/mission\\_canyon/mc\\_multimodal.php](http://longrange.sbcountyplanning.org/planareas/mission_canyon/mc_multimodal.php))



# MISSION PARK TO MISSION CANYON MULTIMODAL IMPROVEMENTS PLAN

DRAFT | DECEMBER 2014



COUNTY OF SANTA BARBARA  
CITY OF SANTA BARBARA

# Acknowledgements

## SANTA BARBARA COUNTY STAFF

### LONG RANGE PLANNING

**Rosie Dyste** • Previous Project Manager/Senior Planner

**Julie Harris** • Project Manager/Senior Planner

**Allen Bell** • Supervising Planner

### PUBLIC WORKS

**Matt Dobberteen** • Alternative Transportation Manager

**Chris Sneddon** • Deputy Director Transportation

**Gary Smart** • Transportation Engineer

## CITY OF SANTA BARBARA STAFF

### PUBLIC WORKS

**Robert Dayton** • Principal Transportation Planner

**Pat Kelly** • Assistant Public Works Director/City Engineer

**Derrick Bailey** • Supervising Transportation Engineer

**Brian D'Amour** • Acting Principal Civil Engineer

**Andrew Grubb** • Project Engineer II

**Laura Yanez** • Project Engineer

## IN-KIND SERVICES

**Mission Heritage Trail Association** (formerly Concerned Citizens for Safe Passage)

Special thanks to members Tom Jacobs, Don Olson and Fred Sweeney

**Santa Barbara Woman's Club**

**Santa Barbara Museum of Natural History**

## GRAPHIC DESIGN

**Bertoux & Company**

## ILLUSTRATIONS

**Fred L. Sweeney, AIA**

## FUNDING SOURCE

This project was funded by a Caltrans Community Based Transportation Planning Grant

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# 1

## Introduction



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# 1.1 Background

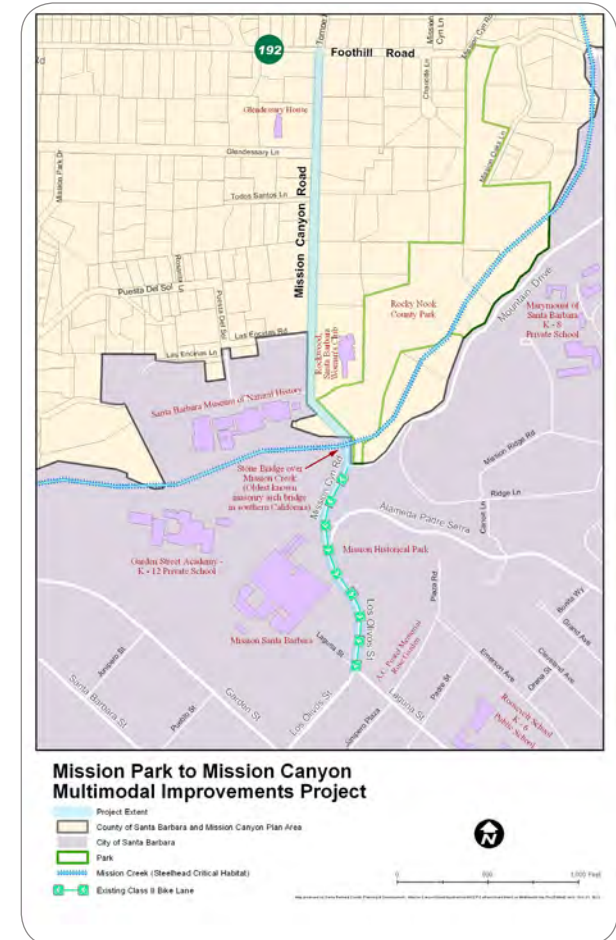
In 2012, Santa Barbara County teamed with the City of Santa Barbara to apply for a Community-Based Transportation Planning Grant from Caltrans for the Mission Park to Mission Canyon Multimodal Improvements Project (project). The project area is where the urban City of Santa Barbara (City) transitions to the unincorporated, semi-rural Mission Canyon area (County) in the foothills of the Santa Ynez Mountains. The project is proposed along an approximately 3/4 mile long, two-lane road starting at the intersection of East Los Olivos Street/Laguna Street in the City to the intersection of Mission Canyon Road/Foothill Road (State Route 192) in the County. East Los Olivos Street becomes Mission Canyon Road at West Mountain Drive. The corridor is the “gateway” into Mission Canyon and is a key emergency evacuation route for adjacent high fire hazard areas. It is also a popular destination for tourists and local residents.

The project is supported by the Mission Heritage Trail Association (MHTA, formerly known as Concerned Citizens for Safe Passage), a citizens group consisting of institutional, civic, and neighborhood associations, and private property stakeholders. MHTA was formed to address concerns about safety and accessibility along this historical and scenic corridor in the interest of public safety and enjoyment of this historic neighborhood. MHTA identified safety issues, formed goals for improvements, and developed potential improvement ideas for further study. The Safe Passage – Mission Historical Park to Mission Canyon Information Packet (June 2012) contains a wealth of

information about the corridor and can be viewed on the following website:  
<http://www.missionheritagetrailassociation.com>.

In response to strong community sentiment and involvement, the County, in partnership with the City, pursued the Caltrans grant to fund the development of concept plans for improvements to the corridor. Caltrans awarded the grant in August 2012. The Caltrans grant is matched by a significant contribution of in-kind services from members of MHTA. In addition, the Santa Barbara Museum of Natural History and the Santa Barbara Woman’s Club provided refreshments and meeting spaces for the public workshops. The project team is comprised of County Long Range Planning and Public Works staff and City Public Works staff.

The concept plans were created through a collaborative government/community process and resulted in plans for an improved circulation system that would be accessible and safe for walkers, bikers, and drivers. The concept plans resulting from this project create a concrete and feasible vision for this corridor. Based on the community input, the concept plans provide a continuous west-side pedestrian pathway linking Old Mission Santa Barbara (Mission), Santa Barbara Museum of Natural History, and Foothill Road (State Route 192). Bicyclist passage is improved up Mission Canyon Road to Foothill Road via designated bike lanes and/or bike areas, depending on the available road width.



# 1.2 Purpose and Need

The corridor is currently difficult to traverse and potentially unsafe for many users. Key factors include heavy traffic (8,700 to 10,700 average daily trips), and multiple, poorly controlled intersections. Disconnected, non-standard pathways and physical constraints force pedestrians to enter and/or cross Mission Canyon Road several times. Physical constraints include historic structures, residences, stone walls, the bridge and “bottleneck” over Mission Creek, mature native trees, signs, and utility poles. These constraints, in combination with high traffic volumes and thousands of visitors, make creating a safe experience for all users challenging.

Some of the most significant problems and deficiencies include:

- Lack of continuous, universally accessible pedestrian pathways;
- Poor condition of existing pedestrian pathways;
- Pedestrian access blocked by parked cars during special events and by poorly placed utilities and signs;
- Poor sight distances and visibility at intersections and crosswalks; and
- Cyclists forced to share heavily traveled road.



## GOAL AND OBJECTIVE:

The goal of this project is a continuous pedestrian walkway and bikepath/bikeway along the corridor that reflects the desires of the community and other interested stakeholders and provides safe access to all users regardless of ability or mode of transportation. The objective is to develop a community-led solution that Santa Barbara County and the City of Santa Barbara would use as a blueprint for funding and implementing future improvements.





# 1.3 Policy Context

The proposed improvements for the Mission Canyon corridor implement federal, state, and local policies ensuring all users of streets, roads, and highways are taken into consideration when developing new or retrofitting existing transportation systems.

The *United States Department of Transportation Policy Statement on Bicycle and Pedestrian Transportation Regulations and Recommendations* supports “fully integrated active transportation networks” that include accommodations for bicyclists and pedestrians. The *California Complete Streets Act* (Assembly Bill 1358) requires cities and counties, upon the next update of their circulation element, to plan for the development of multimodal transportation networks. This network should consist of complete streets designed and constructed to serve all users, regardless of their age or ability, or whether they are driving, walking, biking, or taking transit. Finally, the project directly implements City and County long range goals and policies for the project area as detailed below.

## CITY OF SANTA BARBARA

The City of Santa Barbara’s General Plan (2011) provides a policy basis for this project. Several policies from the Circulation Element and the City’s Pedestrian Master Plan (2006) support the development of multimodal improvements in the corridor. The most relevant policies are as follows:

### CITY OF SANTA BARBARA CIRCULATION ELEMENT (2011)

*Goal: Integrated Multi-Modal Transportation System.*

Create a more integrated multi-modal transportation system to connect people, places, goods, and services. Provide a choice of transportation modes and decrease vehicle traffic congestion.

**Policy 5.1.2** (in part): Identify and link major activity centers and destinations with walkways. This will consist of the following:

- Improving pedestrian access in and around the Mission by providing safe and attractive walking connections between the Mission, Rose Garden, Rocky Nook Park, Natural History Museum, and Alameda Padre Serra.

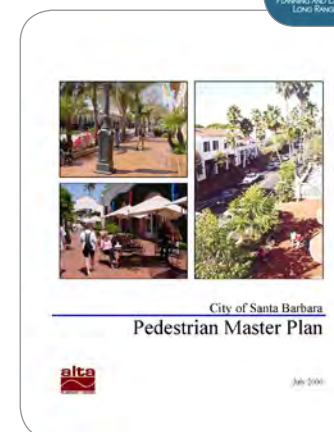
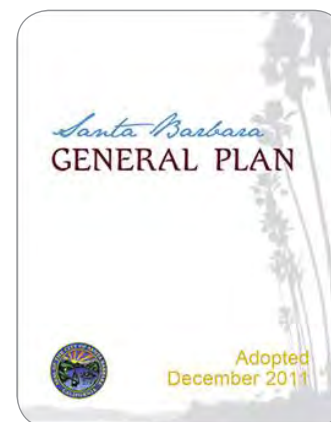
### CITY OF SANTA BARBARA PEDESTRIAN MASTER PLAN (2006)

*Goal 1: Improve the pedestrian system to increase walking in Santa Barbara.*

**Policy 1.1:** The City shall expand the sidewalk network to increase walking for transportation and recreation.

**Policy 1.2:** The City shall improve pedestrian safety and comfort at intersections.

**Policy 1.3:** The City shall enhance pedestrian corridors.



## SANTA BARBARA COUNTY

In 2014, the County Board of Supervisors adopted the Mission Canyon Community Plan (MCCP). Based on City provision of services to Mission Canyon, the MCCP was also approved by the City Santa Barbara City Council in 2014. The MCCP contains several policies and actions in support of the project as follows:

### MISSION CANYON COMMUNITY PLAN

#### Action CIRC-MC-2.2:

The County shall actively pursue siting a pedestrian on-road trail adjacent to Mission Canyon Road from the intersection with Mountain Drive to the Santa Barbara Botanic Garden. Trail design and siting shall be consistent with the semi-rural neighborhood character along Mission Canyon Road.

#### Policy CIRC-MC-3:

The County shall coordinate with the City of Santa Barbara to seek grants and other funding sources to design and implement a plan to improve multimodal access and safety for residents and visitors traveling between Mission Santa Barbara, Rocky Nook Park, Santa Barbara Museum of Natural History, and local schools and neighborhoods within and adjacent to the City/County boundary at Mission Canyon Road, in accordance with the streetscape plan for the Scenic Corridor (see Action VIS-MC-3.2).

Improvement plans for this roadway shall include, but not be limited to, an evaluation of the following issue areas:

- Traffic patterns and on-street parking generated by special events at Mission Santa Barbara, the Santa Barbara Museum of Natural History, and the Santa Barbara Woman's Club;
- Unpermitted encroachments into the public road ROW;
- Dual purpose pedestrian trails/emergency vehicle turnout zones in appropriate locations, including the southbound segment of Mission Canyon Road and Los Olivos Street for use in an emergency evacuation with the aid of traffic control;
- Improvements to the Mission Creek bridge; and
- Historical resources evaluation by the City and County Historic Landmarks Commission/Committee.

#### Action CIRC-MC-3.1:

The County shall collaborate with the community and City of Santa Barbara to improve multimodal connections between Mission Santa Barbara, Rocky Nook Park, Santa Barbara Museum of Natural History, and local schools and neighborhoods adjacent to the City/County boundary at lower Mission Canyon Road and Los Olivos Street, consistent with the historical and scenic nature of this area.

#### Policy VIS-MC-3.2:

The County shall coordinate with the City of Santa Barbara, area non-profit institutions, and residents to seek grants and other funding sources to design and implement the Phase II streetscape plan for the Mission Canyon Scenic Corridor, coordinated with multimodal access and safety improvement plans (see Policy CIRC-MC-3).

Aesthetic considerations for the streetscape plan could include the following programs:

- Design and materials for paths, crosswalks, and streetscape features;
- Protection of existing stone walls and other historic features;
- A signage plan;
- Landscaping recommendations; and
- Undergrounding utilities.

# 1.4 Public Outreach

Public participation and support are crucial to the success of this project. Past experience has shown that public controversy and opposition can stifle project proposals. The project area is highly visible and sensitive for the community; therefore, general community consensus around the concept plans was of the utmost importance. The public outreach strategy included extensive notification and workshops.

## PUBLIC NOTIFICATION

Due to the number and variety of stakeholders, the project team coordinated community outreach efforts with the MHTA volunteers. Community presentations conducted by MHTA prior to project initiation and during the project dovetailed with the outreach for the project and built upon the contacts already made by MHTA.

The following methods were used to ensure stakeholder participation, expand outreach, and solicit feedback.

## PROJECT WEBPAGE AND SOCIAL MEDIA

A webpage was created for the project linked to the County's Mission Canyon Community Plan webpage: [http://longrange.sbcountyplanning.org/planareas/mission\\_canyon/mc\\_multimodal.php](http://longrange.sbcountyplanning.org/planareas/mission_canyon/mc_multimodal.php). The webpage provided information about meeting dates, locations, and materials and links to the Mission Canyon Corridor concept plans. Long Range Planning's Facebook page (<https://www.facebook.com/longrange.sbcounty>) was used to announce public workshops and hearings.



## INTERESTED PARTIES LIST

The County's existing Mission Canyon interested parties email list was used and interested community members were added prior to and after each community workshop. The email notification list was then expanded to include key members of non-profit organizations; including the Chamber of Commerce, Downtown Organization, and Visitors Bureau. Chumash representatives were included on the email list to ensure opportunities for input and participation.

## SELF GUIDED TOUR BOOKLET

A self-guided tour booklet was created to provide a guide for interested citizens to use before or after attending public workshops (Appendix A). The booklet was also made available on the project webpage.

## PUBLIC WORKSHOPS

Public workshops were the cornerstones of this project and were planned carefully to maximize

public input and participation. Significant in-kind donations were provided for the workshops; including auditorium space from the Santa Barbara Woman's Club and refreshments from the Santa Barbara Museum of Natural History and MHTA members. Workshops were announced with flyers and emails to interested parties, notices in neighborhood association newsletters, and on Long Range Planning's Facebook page. The workshops are summarized below.

## PUBLIC WORKSHOP #1

Workshop 1 was held in October 2013 and was attended by approximately 50 – 60 people. The format included a staff presentation and breakout/working sessions with two facilitators and eight to ten participants at each table. It provided an opportunity for the community to answer the following questions about the corridor:

- What is important to you about the Mission Canyon Corridor?
- What works well now about the Mission Canyon Corridor?
- What's not working well about the Mission Canyon Corridor?
- What needs to be fixed?
- What needs to be left alone?

Workshop tools included large scale maps that participants were encouraged to write and draw on and a slide show of project area photographs.

Participants were invited to take a self-guided walking/cycling tour and provided with a self-guided tour booklet. They were also encouraged to sign up for walking tours guided by the MHTA.

## PUBLIC WORKSHOP #2

The feedback and comments collected from the first workshop provided the fundamental basis for the project team to determine community consensus and prepare concept plans for the corridor for presentation and further feedback at a second public workshop. In addition to the concept plans, the project team prepared a list of other features the community wanted addressed. These other features are considered long-term concepts to be considered at a future date because they are more

complex and/or potentially controversial than the concept plans. The time necessary to develop solutions and reach public consensus would exceed that available under the current grant and scope of work.

Workshop 2 was held in April 2014 and was attended by approximately 90 people. The format included staff presentations of the concept plans, questions and answers, and participants voting. After the presentation and question and answer period, participants were asked to vote on the concept plans with a green, yellow, or red dot to signify their support. Green signified support, yellow signified that concept was not perfect but probably could accept, and red signified no support.

The results were as follows:

WORKSHOP #2: CONTINUOUS WEST-SIDE PATHWAY CONCEPT PLANS VOTE RESULTS			
SUPPORT	UNDECIDED	NO SUPPORT	TOTAL
56	5	2	63
89%	8%	3%	100%





Workshop participants were also asked to vote on the potential long term projects, described as issues that participants at the first public workshop stated needed fixing but did not meet the City and County’s criteria as an achievable plan. The results are presented in the table to the right.

The votes provided the impetus for the project team to consider the concept plans for the corridor described in Chapter 3 as the “community consensus” proposal. The long term concepts are included in Chapter 3 as potential future projects for the corridor.

WORKSHOP #2: LONG TERM PROJECTS VOTE RESULTS				
PROJECT	SUPPORT	UNDECIDED	NO SUPPORT	TOTAL
Add Left Turn Lane Las Encinas Road	7	11	11	29
Reconfigure Laguna and Los Olivos Intersection	13	10	11	34
Improve APS Intersection	30	7	5	42
Reconstruct Mountain Drive Intersection	19	11	2	32
Increase visibility at private drives and driveways	6	6	16	28
Continuous east side pathway	5	2	17	24
Underground utilities	33	0	1	34
Rapid flashing beacons	14	6	12	32
Widen bridge over Mission Creek	0	1	29	30
Increase lighting	5	2	20	27

WORKSHOP #2: CONCEPT PLANS FOR CONTINUOUS WEST-SIDE PATHWAY



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# 2

## Existing Settings and Conditions



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## 2.1 Project Location

The project area is about three miles from the coastline, at the foothills of the Santa Ynez Mountains. This unique area is one of the key attractions of the South Coast and is characterized by a blend of historic landmarks, institutional uses, single-family homes, and great natural beauty. The project area is an asset to the local community and to the greater Santa Barbara region.

The corridor is located in a transition area between the City and the County. The southern half of the corridor includes Old Mission Santa Barbara (Mission), Mission Historical Park, A.C. Postel Rose Garden, and the Santa Barbara Museum of Natural History (museum). Where the City's jurisdiction ends and the County jurisdiction begins is the unincorporated Mission Canyon Community Plan Area. The Mission Canyon Plan Area portion of the corridor consists of single-family homes, Rocky Nook County Park, and the Santa Barbara Woman's Club.





## 2.2 Character and Regional Importance

The corridor is a key historic, scenic, cultural and recreational area. It also serves as an important access route to Mission Canyon neighborhoods to the north. It is characterized by the following traits.

### BEAUTY

Rustic Spanish Revival-style structures lining the corridor are complimented by the A.C. Postel Memorial Rose Garden, Mission Historical Park, Rocky Nook Park, a historic olive grove, Mission Creek, and a canopy of mature oak and sycamore trees that creates a tranquil, rural feel. Views of the expansive Pacific Ocean dominate the horizon to the south, while northbound travelers are greeted with views of the nearby Santa Ynez Mountains.

### BIOLOGY

The corridor traverses Mission Creek which is designated critical habitat for the endangered Southern California steelhead trout. Mission Creek and Rocky Nook Park contain significant stands of coast live oak riparian forest and portions of the corridor are lined with mature native oak and sycamore trees. Rocky Nook Park contains high quality and diverse habitat for special status birds species documented during recent surveys, including oak titmouse, yellow warbler, and Nuttall's woodpecker.

### HISTORY

The project area has a rich history of Chumash and Spanish settlement. Archeological sites and historical structures infuse the area with a palpable

sense of heritage. The area also includes the oldest known masonry bridge in southern California, which traverses Mission Creek.

### GLOBAL DESTINATIONS

The corridor receives approximately one million visitors per year, as it includes two of Santa Barbara's chief attractions: Old Mission Santa Barbara and the Santa Barbara Museum of Natural History.

### COMMUNITY

Many of the structures along the corridor are key gathering places for informal relaxation, education, and special events. The corridor is the site of three annual celebrations that are integral to the Santa Barbara community: Fiesta/Old Spanish Days, the Italian Street Painting Festival (both pictured to the right), and the Santa Barbara Wine Festival. Several schools are located within approximately one mile of the corridor, so it also serves as a hub for walking field trips throughout the year.

### FIRE SAFETY

Despite all the festivity, the viability and efficiency of transportation along the corridor is also essential to the safety of Mission Canyon and the surrounding community. The project area is within the Very High Fire Hazard Severity Zone, and has a significant history of wildfires. The corridor serves as a key evacuation route for many residential areas within and adjacent to Mission Canyon.



## 2.3 Features

### OLD MISSION SANTA BARBARA

Considered the “queen of the missions,” Old Mission Santa Barbara is touted by the National Park Service as “One of the finest and most distinguished of the 21 California missions from an architectural standpoint.” Mission Santa Barbara was founded in 1786 by the Spanish Franciscans as the tenth Californian mission. Today, it is home to a large and active community of Franciscan friars and houses a retreat center, museum, and 10-acre garden. It is situated on a 13-acre parcel at the gateway to the corridor, on the west side of the Laguna Street/ East Los Olivos intersection, within City limits. The Mission’s property contains a parking lot, and visitors also access the site via walking, biking, and transit. The Mission receives approximately 750,000 to 1 million visitors per year, not including those who attend for regular religious services or for major events, such as Fiesta.

### MOUNT CALVARY MONASTERY (AT ST. MARY’S RETREAT HOUSE)

This Benedictine retreat house and monastery is situated on a 4.8-acre property just above Mission Santa Barbara and below Mission Creek, within City limits. The property contains two main houses, one built in 1885 and the other in 1917. It is home to a community of Anglican monks who relocated to this facility when their property was destroyed in the 2008 Tea Fire. The house accommodates up to 26 guests, and its grounds include landscaped gardens, a chapel, a labyrinth, and a library (Mount Calvary).



### SANTA BARBARA MUSEUM OF NATURAL HISTORY

Founded in 1916, the museum comprises a cluster of Spanish Revival-style buildings nestled into 17 acres of riparian oak woodland, just above Mount Calvary Monastery and Mission Creek. The museum is a regional and global attraction that is home to more than three million objects, including the largest collection of Chumash Native American artifacts in the world. Displays of regional animal life, a planetarium, and an ethnobotanic garden attract as many as 196,000 visitors per year (maximum recorded attendance), including thousands of K–12 students on fieldtrips.



The museum is also a prominent center for research, as its unique and extensive collections draw scientists from all over the world. The museum is within the City of Santa Barbara. Visitors use the project area to access the museum via car, transit, walking, and biking.

### WOMAN’S CLUB

In 1927, the Santa Barbara Woman’s Club commissioned the construction of a Spanish Revival-style clubhouse, set back from Mission Canyon Road just above Rocky Nook Park, to serve as the location for meetings and events. The 2.9-acre parcel is within County jurisdiction and is beautifully landscaped with oaks and ornamental plantings. The facility attracts about 14,000 visitors per year for events and activities. The clubhouse has an adjacent parking lot and can also be accessed via walking, biking, and transit.





## SCHOOLS

Students from five nearby schools often participate in walking fieldtrips through the corridor to the Mission and/or museum. Students typically walk in groups of 30–35 with their teachers and volunteer parents. Because the resources along the corridor have both local and regional interest, hundreds of buses with students on fieldtrips arrive at the Mission and/or museum each school year.



MARYMOUNT OF SANTA BARBARA

TABLE 1: SCHOOLS NEAR THE MISSION CANYON CORRIDOR			
NAME	APPROX. DISTANCE FROM CORRIDOR	GRADE LEVELS	ENROLLMENT
Roosevelt Elementary School	660 feet	K-6	583
Garden Street Academy	800 feet	K-12	132
Marymount of Santa Barbara	1,700 feet	K-8	215
Peabody Charter Elementary School	1 mile	K-6	750
Santa Barbara Middle School	1 mile	6-9	122

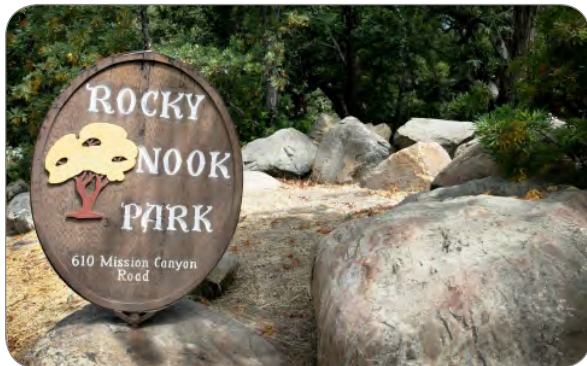


ROOSEVELT ELEMENTARY SCHOOL

## PARKS

### ROCKY NOOK PARK

This 19-acre County-owned park has a tree cover of oaks and sycamores, with interspersed sandstone boulders. Approximately 450 feet of the parkland, including the park entrance, fronts the corridor. Park amenities include a playground, trails, picnic tables, and barbeque grills.



### MISSION HISTORICAL PARK

This 10-acre City-owned park contains a variety of ruins that date to the Mission period and is landscaped with a historic olive grove, a rose garden, and an expanse of lawn that has views of the Pacific ocean. Approximately 960 feet of parkland fronts the corridor.



The northern 2-acre portion of the park is separated from the southern portion by Alameda Padre Serra, a major access road. This upper portion has scattered oaks and sycamores and is the site of a pottery building, mill, and two reservoirs that were built during the Mission period and were formerly functioning pieces of the Mission Santa Barbara infrastructure. A historic olive grove dedicated to World War I veterans is located in the lower corner of this 2-acre portion, fronting both East Los Olivos Street and Alameda Padre Serra.



The 8-acre, southerly portion of the park was originally owned by the Franciscan Friars and is located directly across from the Mission. The upper portion of this section has scattered oaks and sycamores and contains a tanning vat that dates to the Mission period. This northern section is separated from the expansive grassy lawn below by a portion of the Mission aqueduct, built in 1808.

#### A.C. POSTEL MEMORIAL ROSE GARDEN

In the southern corner of Mission Historical Park, an array of accredited, All American roses are situated in brick planters around a fountain. Scents, vibrant colors, views of the Mission, and sounds from the fountain make the garden a tranquil, rustic place to wander.



ROSE GARDEN

#### INFRASTRUCTURE

Infrastructure located along the corridor consists of the following items:

- **Street Lights:** Street lights are located at the major intersections. Several lights are also located mid-block on Mission Canyon Road.
- **Drainage:** Stormwater runoff is informally channeled in the shoulder along Mission Canyon Road. There is a large storm drain on the west side of Mission Canyon Road near Rocky Nook Park.
- **Power Lines:** Electrical power lines are not visible in the lower portion of the corridor until the APS triangle. Electrical power lines continue on both sides of the corridor after the Mission Creek bridge.



POWER LINES AND STREET LIGHTS



WALKWAY AND DRAINAGE ALONG MISSION CANYON ROAD



POWER LINES



## 2.4 Historical Resources

Many of the South Coast region's key historical resources are within or adjacent to the corridor. Some of these resources have been given official landmark or historical merit designations at the local, state, or national level. Others do not have official designations but are considered key aspects in the character of the corridor. Officially designated resources are listed in Table 2 on page 2-8.

The official designations each have different definitions, which are as follows:

### NATIONAL HISTORIC LANDMARK

"Nationally significant historic places are designated by the Secretary of the Interior because they possess exceptional value or quality in illustrating or interpreting the heritage of the United States." There are just over 2,500 places with this designation (National Park Service 2014).

### NATIONAL REGISTER OF HISTORIC PLACES

"The official list of the Nation's historic places worthy of preservation," and a part of a national program to "identify, evaluate, and protect America's historic and archeological resources." National Historic Landmarks are added to the National Register of Historic Places when they are officially designated (National Park Service 2013).

### CALIFORNIA HISTORICAL LANDMARK

A building, structure, site, or place that has been designated by the Director of California State Parks due to its statewide historical significance. The

resource must meet one of three criteria: it is the first, last, or only example of its type; it is associated with a historically influential person or group; or it is an exemplar of an influential architect's work, an architectural movement, a period, or a style (California State Parks Office of Historic Preservation 2013).

### COUNTY OF SANTA BARBARA LANDMARK

A building, structure, site, place, or object that has historical, aesthetic, or other special character or interest and designated as a Landmark under the provision of County Code Chapter 18A (County of Santa Barbara 1996).

### CITY OF SANTA BARBARA LANDMARK

A structure, natural feature, site or area having historic, architectural, archaeological, cultural or aesthetic significance and designated as a landmark under the provision of Municipal Code Chapter 22.22 (City of Santa Barbara Historic Resources Element 2012).

### CITY OF SANTA BARBARA STRUCTURE OF MERIT

Although not a City Landmark, these structures have cultural, architectural, archaeological, or aesthetic significance and are designated by the City Council, subsequent to a recommendation by the City's Historic Landmarks Commission (City of Santa Barbara 2008).

### OTHER RESOURCES

Other resources that are potentially historic but not officially designated are as follows:

#### ST. MARY'S RETREAT HOUSE PROPERTY

Contains two houses, one built in 1885 and the other in 1917. The family who commissioned these houses played a role in the beginnings of the Museum.





#### SANDSTONE WALLS

Sandstone walls dating to the late 19th century border or front the Mission property, the Saint Mary's Retreat House property, the museum, and the property at 609 Mission Canyon Road, located between Saint Mary's and the museum.

#### ROCKWOOD

This is a Spanish Revival building, constructed in 1928, and the home of the Santa Barbara Woman's Club.

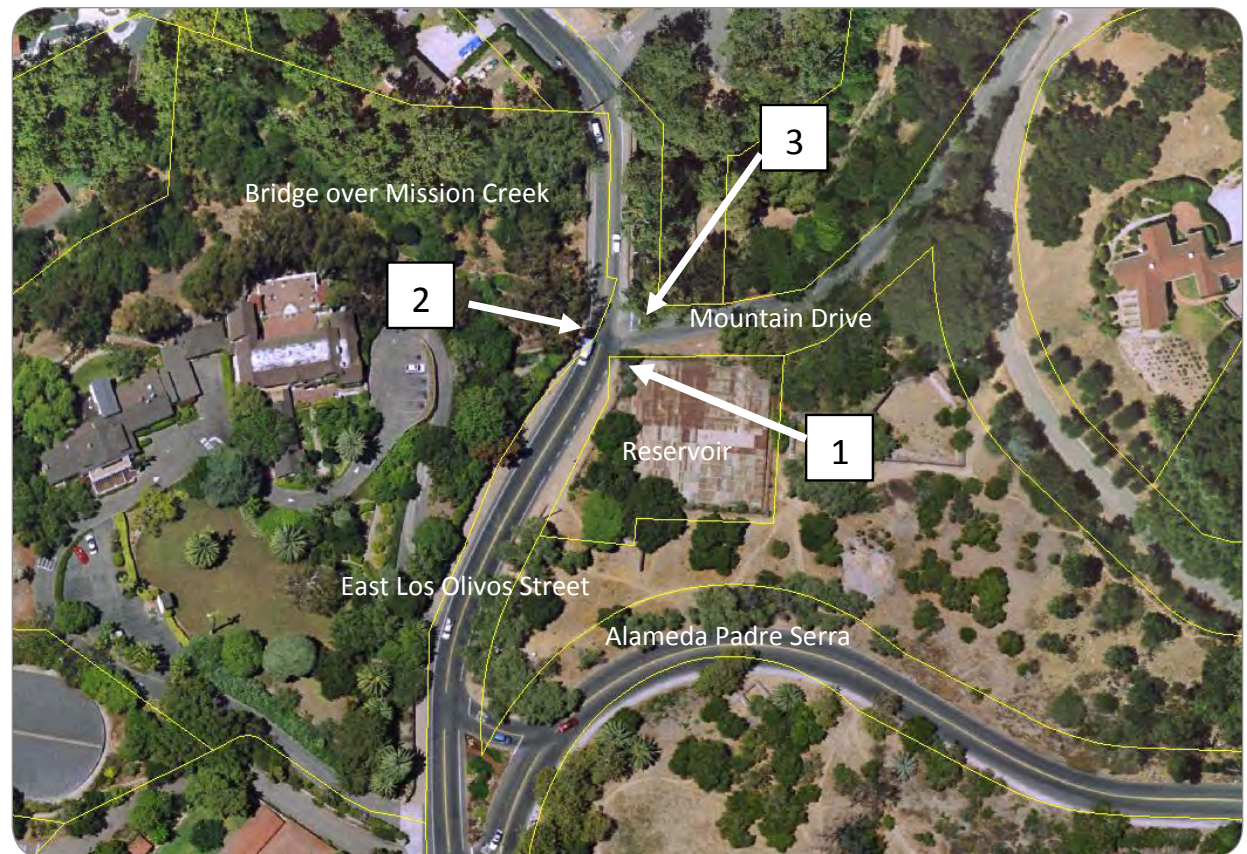
**TABLE 2: HISTORIC RESOURCES IN THE MISSION CANYON CORRIDOR**

	RESOURCE	DESIGNATIONS	INFORMATION
	Old Mission Santa Barbara	<ul style="list-style-type: none"> <li>• National Historic Landmark District</li> <li>• National Historic Landmark</li> <li>• National Register of Historic Places</li> <li>• California Historical Landmark</li> <li>• City of Santa Barbara Landmark</li> </ul>	Includes church, cemetery, garden, fountain, and remains of pottery, tanning vats, stone buildings, aqueduct segments and other features in Mission Historical Park.
	Mission Historical Park	<ul style="list-style-type: none"> <li>• National Register of Historic Places</li> <li>• California Historical Landmark</li> <li>• City of Santa Barbara Landmark</li> </ul>	Includes City-owned park and all Mission period structures such as reservoir, Cota Sycamore tree, aqueduct segments, 1891 stone bridge over Mission Creek, and 1910 Oliver trough-fountain.
	Santa Barbara Museum of Natural History	<ul style="list-style-type: none"> <li>• City of Santa Barbara Structure of Merit</li> </ul>	Includes buildings, courtyard, and stone wall border.
	Glendessary House	<ul style="list-style-type: none"> <li>• County of Santa Barbara Landmark</li> </ul>	A Tudor Revival-style mansion used as private residence.

## 2.5 Physical Constraints

The corridor has a high concentration of historic resources, including, but not limited to, the 1891 stone bridge over Mission Creek and traces of the Mission aqueduct, including the “pillar” or wall section on both sides of the corridor near Mountain Drive.

These resources limit potential solutions for pedestrian safety because they form “pinch” points along the corridor between the historic City reservoir to the bridge over Mission Creek. The map below depicts the location of the three pinch points that constrain access. They are described in detail on the following pages.



PRIMARY CONSTRAINTS (PINCH POINTS) TO PEDESTRIAN ACCESS

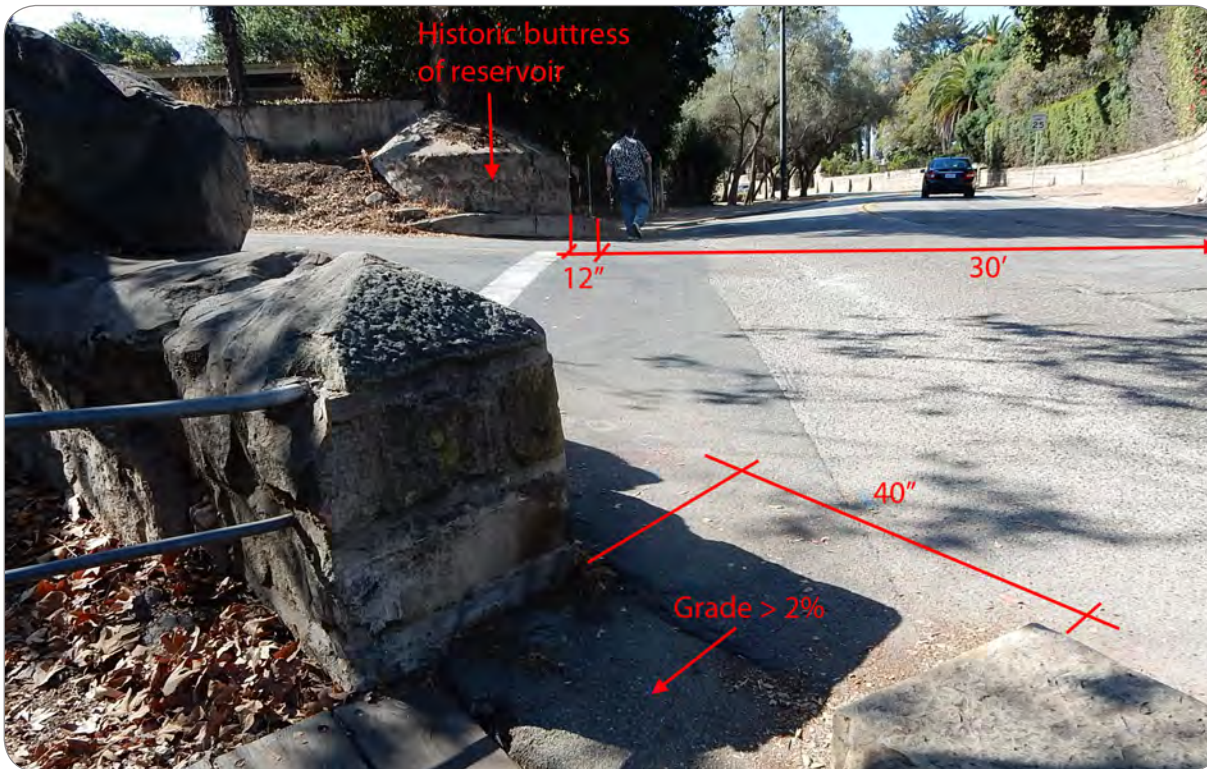


### PINCH POINT #1

The first pinch point is where pedestrians approach the Mountain Drive/Mission Canyon Road intersection on the east side of Mission Canyon Road. There is a constraint here for pedestrians and vehicles using the same space due to the location of the historic reservoir structure to the east and the location of the “pillar” or the wall section of the main Mission aqueduct on the west.

The constraint occurs because as a pedestrian walks parallel to the reservoir wall along the east side of Mission Canyon Road, the available walking area narrows from approximately 5 feet to 12 inches (the space available between the reservoir stone abutment and the concrete curb). Where it narrows, pedestrians are forced to step into the traffic lane to cross the Mountain Drive intersection.

The width of the road from this point to the west is approximately 30 feet which accommodates a standard two lane road with a bike lane each way. On the west side, the available walking area is less than 4 feet wide (ADA requires a 48-inch clearance) from the inside of the street curb to the historic Mission Aqueduct “pillar”/wall. However, before pedestrians get to this location, there are two signs prohibiting walking on this side of the road.





## PINCH POINT #2

The second pinch point occurs approximately 30 feet to the north of the Mission Aqueduct “pillar”/wall on the west side of Mission Canyon Road. Because of this pinch point, pedestrians are currently prohibited from walking on this side of the road.

Parallel to the corridor at this location is a stone wall on the property line of the Mt. Calvary Monastery. At the northerly portion of this wall section, the wall ends at a junction with the street curb just before the historic stone bridge abutment. There is approximately 8 inches of clearance between the end of the wall’s stone column and the street curb.

The pathway width in this section goes from 4 feet at the Mission aqueduct “pillar” to 8 inches at the end of the Monastery wall as it converges with the Mission Creek bridge. Pedestrians cannot cross the bridge on the west side without walking in the travel lane.





### PINCH POINT #3

The third pinch point is located on the east side of the corridor to the immediate north side of Mountain Drive as it meets East Los Olivos Street/ Mission Canyon Road. An historic 1910 rock water trough is located at the edge of the road pavement at the exact corner of the intersection. The historic water trough does not allow for adequate crosswalk striping across Mountain Drive to the southerly corner of the historic reservoir stone wall and abutment. Pedestrians are required to walk in the right turn lane for Mountain Drive vehicles and also into the traffic/bike lane of the corridor.



## 2.6 Circulation

East Los Olivos Street and Mission Canyon Road are two-lane, north-south trending roadways that serve residences, institutions, recreational amenities, and through traffic. The corridor of East Los Olivos Street and Mission Canyon Road is the primary entrance into Mission Canyon. The speed limit of East Los Olivos Street is 25 miles per hour (mph), slowing to 20 mph at the “s” curve where the road crosses Mission Creek, and then increases to 35 mph on Mission Canyon Road. The corridor is designated by the City and County as a “No Parking Zone.” The most recent traffic counts taken by the City and County in 2011 and 2012 respectively show 10,600 average daily trips (ADT) near the bridge over Mission Creek, 9,260 ADT south of Puesta del Sol, and 7,958 ADT south of Foothill Road.

### INTERSECTIONS

There are eight intersections along the corridor and, except for the intersection with Foothill Road, each intersecting road terminates on the corridor. Reducing the number of pedestrian crossings at intersections is a key focus of this project. Poor visibility, road curvature, and lack of designated sidewalks/walkways create intersections that are difficult and often hazardous for pedestrians, bicyclists, and vehicles to cross. The following list describes the intersections, beginning from the south end of the corridor.

#### 1. EAST LOS OLIVOS STREET / LAGUNA STREET

This intersection is located just south of the Mission and is controlled by a four-way stop. Pedestrians

can cross each of the four streets that feed into this intersection using painted crosswalks. Laguna Street ends at the Mission.



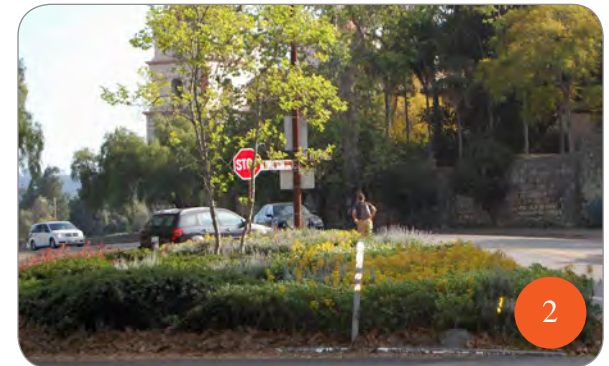
#### 2. EAST LOS OLIVOS STREET / ALAMEDA PADRE SERRA (APS)

The intersection of APS and East Los Olivos Street is known as the APS triangle. At the triangle, the APS is divided into two roadways just before it intersects with East Los Olivos Street. Much of the APS triangle is difficult for pedestrians, bicycles, and vehicles to navigate due to its unique configuration. There are no crosswalks. In the past, the City proposed a roundabout at this intersection but did not pursue it further due to lack of community support.

#### 3. EAST LOS OLIVOS STREET / MISSION CANYON ROAD / WEST MOUNTAIN DRIVE

West Mountain Drive approaches the corridor steeply from the east and ends at a stop sign where East Los Olivos Street becomes Mission Canyon Road. The intersection can be difficult for

pedestrians, bicyclists, and drivers to navigate. The curvature of East Los Olivos Street/Mission Canyon Road obstructs visibility and the steep incline can make it difficult for drivers to see oncoming traffic and pedestrians crossing West Mountain Drive.



**4. PUESTA DEL SOL/MISSION CANYON ROAD**  
Puesta Del Sol is a two-lane, one-way street that approaches Mission Canyon Road from the east and ends at this intersection. Both lanes of Puesta Del Sol have stop signs at this intersection.



On the east side of this intersection is an entrance to the Santa Barbara Woman's Club. There is a crosswalk at the intersection across Mission Canyon Road and a wooden sign at the east end of the crosswalk stating "Crosswalk to Museum."

#### 5. LAS ENCINAS ROAD / MISSION CANYON ROAD

Las Encinas Road is a two-way street that approaches Mission Canyon Road from the west and ends at this intersection. A stop sign controls traffic turning from the eastbound lane of Las Encinas Road onto Mission Canyon Road. Many cars traveling north on Mission Canyon Road turn left onto Las Encinas Road to access the Santa Barbara Museum of Natural History and/or the San Roque neighborhood to the west. There is no left turn lane but the road widens here and cars traveling north on Mission Canyon Road often pass on the right. There are two crosswalks across Mission Canyon Road, one at the north side of the intersection and one at the south side of the intersection.



#### 6. TODOS SANTOS LANE / MISSION CANYON ROAD

Todos Santos is a private, dead-end roadway approaching Mission Canyon Road from the west and terminating at this intersection. There are no crosswalks or stop signs at this intersection.



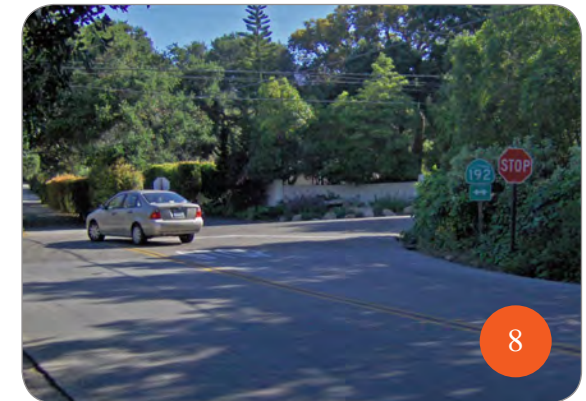
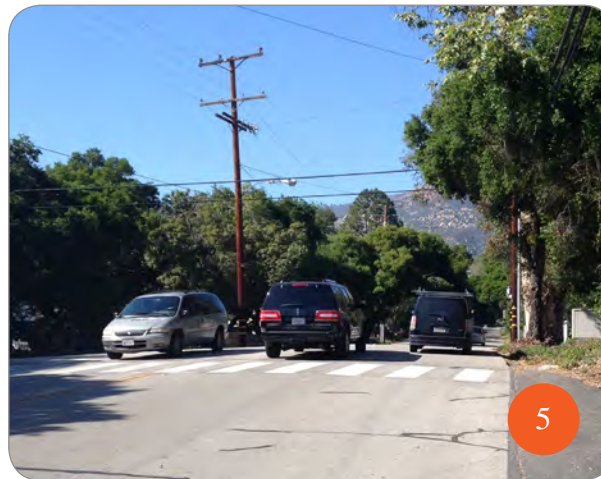
#### 7. GLENDESSARY LANE / MISSION CANYON ROAD

Glendessary Lane is a private roadway approaching Mission Canyon Road from the west and terminating at this intersection. This intersection has one stop sign, which controls traffic turning from Glendessary Lane onto Mission Canyon Road.



#### 8. FOOTHILL ROAD (STATE ROUTE 192) / MISSION CANYON ROAD

This intersection marks the terminus of the project area. This intersection is controlled by a four-way stop.





## OTHER FEATURES

### MID-ROAD CROSSWALK ON EAST LOS OLIVOS STREET

This mid-road crosswalk is about 550 feet north of the East Los Olivos Street/Laguna Street intersection and connects the Mission with Mission Historical Park. There was a bus stop on both sides of the crosswalk but the line was terminated in August 2014 due to lack of ridership.

### ENTRANCE TO ROCKY NOOK PARK

The entrance to Rocky Nook Park is located on the east side of Mission Canyon Road, just north of the bridge over Mission Creek.



ENTRANCE TO ROCKY NOOK PARK

### MISSION CREEK BRIDGE

The bridge over Mission Creek demarcates the boundary between the City and County. The east side of the bridge has an attached wooden pedestrian walkway separated from vehicle traffic by a stone wall. There is no pedestrian crossing on the west side.

## TRAFFIC ACCIDENTS

Improved safety for pedestrians, bicyclists, and motorists is one of the key reasons for pursuing multimodal improvements along this corridor. Ten-year accident histories are available for the segments within the City and County, shown below.

### CITY OF SANTA BARBARA

Within the City's jurisdiction, there were 26 recorded accidents along the corridor between 2002 and 2012, discussed below by intersection or proximity to intersection.

- **East Los Olivos Street and Laguna**  
Five accidents occurred in this area during the above time frame. Two involved collisions between a vehicle and a pedestrian or bicyclist, resulting in injuries. The others involved property damage due to rear end and head on collisions and one car over-turned and hit a fixed object.
- **East Los Olivos Street and Alameda Padre Serra**  
Twenty one accidents occurred in this area during the above time frame, mostly involving property damage between vehicles due to moving traffic violations and several resulting in injuries. One involved a broadside collision with a bicycle, resulting in an injury.

### SANTA BARBARA COUNTY

There were 30 recorded accidents along the corridor between September 2001 and March 2011.



ACCIDENT DAMAGE TO MISSION CREEK BRIDGE

- **Mission Canyon Road Near West Mountain Drive**  
Four accidents occurred in this area during the above time frame. Each accident involved a driver losing control of his or her vehicle and striking a fixed object. The objects that were hit include a fence, rock wall, concrete retaining wall, and power pole.
- **Mission Canyon Road Near Puesta Del Sol**  
Three accidents occurred in this area during the above time frame. Two accidents involved a vehicle striking a fixed object (a telephone pole, signs, and a tree). The third accident involved a vehicle striking a bicyclist when the bicyclist lost control of his or her bicycle.

- **Mission Canyon Road Near Las Encinas Road**  
Nine accidents occurred in this area during the above time frame. Two accidents involved a vehicle striking a fixed object (a rock wall and a pole). Three accidents involved rear-ends, with one involving three vehicles. One accident was caused when a vehicle turning west onto Las Encinas Road struck a vehicle that was stopped at the stop sign on Las Encinas Road. Another accident was caused when a vehicle stopped at one of the crosswalks on the Las Encinas Road intersection for a group of pedestrians and was rear-ended by a second vehicle whose driver did not see the other vehicle stopped at the crosswalk.

- **Mission Canyon Road Near Foothill Road (State Route 192)**  
Fourteen accidents occurred in this area during the above time frame. Eight of these accidents involved single-vehicle collisions with a fixed object. Three involved rear-ends. One involved a vehicle colliding with a horse and rider, resulting in an injury to the horse. One involved a broadside, and another involved an overturned vehicle.

## ALTERNATIVE TRANSPORTATION

### TRANSIT

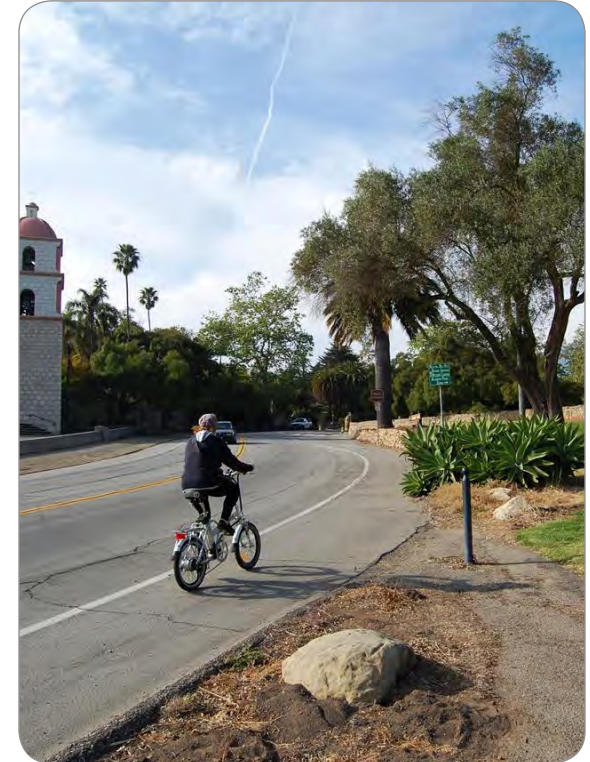
Until recently, the corridor was served by Metropolitan Transit District (MTD) bus line 22 – Old Mission. The line was cancelled in August 2014 due to consistently low ridership.

### BIKE PATHS

There is an existing Class II bike lane on East Los Olivos that ends at the bridge over Mission Creek.

### PEDESTRIAN PATHWAYS / SIDEWALKS

East Los Olivos Street has existing pedestrian pathways of various materials and condition on both sides of the road leading up to the APS triangle. There is a small segment of sidewalk from the mid-block crosswalk north of the Mission to the APS triangle. North of the Mission Creek bridge, there is an informal dirt pathway on the east side of the corridor up to Las Encinas Road. At this point, pedestrians need to cross to the west side and continue on an informal pathway in the road shoulder up to Foothill Road. Rocky Nook Park has a trail leading through the park to the north and to County Fire Station 15 on Foothill Road.



BIKE LANE AT EAST LOS OLIVOS STREET



SOUTHBOUND MISSION CANYON ROAD



MISSION CANYON ROAD NEAR ROCKY NOOK PARK

# 3

## Community Recommended Concept Plans



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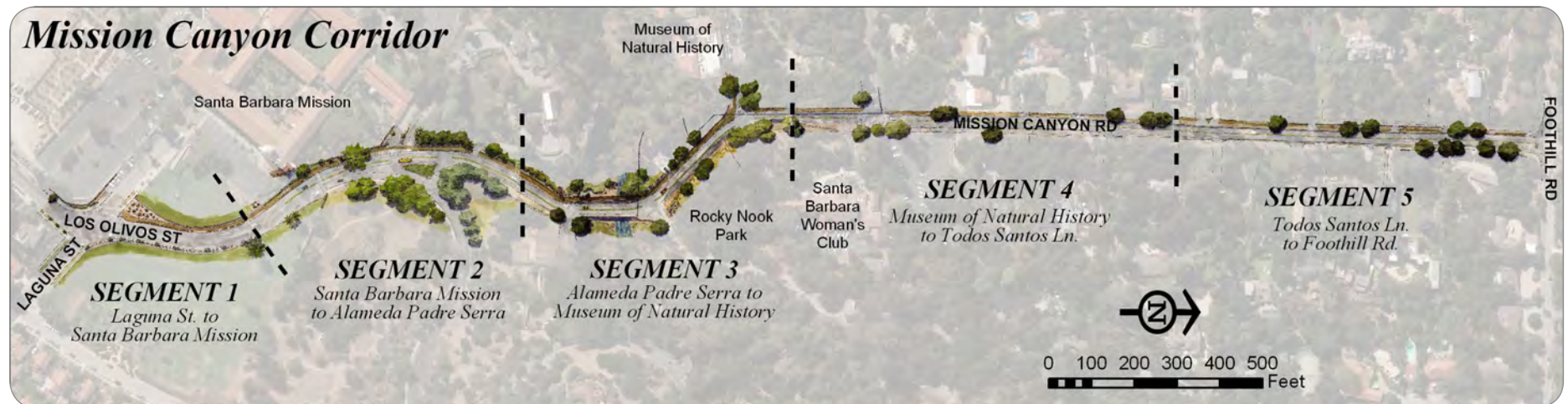
# 3.1 Five Segments of the Corridor

## INTRODUCTION

This chapter discusses the proposed concept plans for improved pedestrian, bicycle, and vehicle circulation. The concept plans were prepared based on public feedback from the first community workshop. The feedback directed an overarching goal for a continuous pathway on the west side of the corridor to eliminate multiple road and intersection crossings, while improving safety for bicyclists and vehicles. While the concept plans primarily improve access between the Mission and the Santa Barbara Museum of Natural History, pedestrians can also use a crosswalk at Puesta del Sol to access Rocky Nook Park (on the east side of the corridor) or continue on the west side of the corridor to the intersection with Foothill Road/State Route 192.

To determine if a continuous west side pedestrian pathway was physically feasible, City and County Public Works engineers reviewed surveys and developed concepts based on physical constraints within the corridor, engineering specifications, and standards for Americans with Disabilities Act (ADA) accessibility. Based on this preliminary assessment, the primary goal of a continuous pathway can be met on the west side of the corridor, subject to further review, assessment of potential impacts to historic and biological resources, and agreement from affected property owners. The City prepared a report that provides the technical background of this work effort, which can be found on the project's website, [http://longrange.sbcountyplanning.org/planareas/mission\\_canyon/mc\\_multimodal.php](http://longrange.sbcountyplanning.org/planareas/mission_canyon/mc_multimodal.php).

To illustrate the concepts, the corridor was separated into five segments. Plan view concept plans and cross sections were prepared by the Mission Heritage Trail Association volunteers. These concepts were shared with the public at the second community workshop and received overwhelming support from workshop participants.



## 3.2 Segment #1: East Los Olivos St./Laguna St. to the Mission

### SEGMENT #1

East Los Olivos Street from the intersection with Laguna Street to northeast corner of Mission Santa Barbara.

This first segment of the corridor is entirely within the City's jurisdiction. There are existing Class II bike lanes and unimproved pedestrian pathways on both sides of the road. The concept for this segment is an ADA accessible, six-foot wide pedestrian pathway on the west side with a landscaped buffer between the pathway and the road. No road realignment or property owner easements would be required to complete the pathway within the existing right-of-way (ROW).



ROAD CROSS-SECTION AT MISSION SANTA BARBARA

## 3.3 Segment #2: East Los Olivos Street to North of APS

### SEGMENT #2

#### East Los Olivos Street from the Mission to Alameda Padre Serra (APS)

This second segment of the corridor is entirely within the City's jurisdiction. There are existing Class II bike lanes and unimproved pedestrian pathways on both sides of the road. The concept for this segment would continue the pedestrian pathway on the west side of the corridor. Landscaped buffers would be included where they can fit within the existing ROW. Private driveway slopes on the west side would be reduced for ADA accessibility. No road realignment or easements would be required to complete the pathway within the existing ROW.

Currently, pedestrians are prohibited from using the west side of the corridor in this segment just past the intersection with APS because the road narrows at the bridge over Mission Creek and there is no pedestrian access on the west side of the bridge. This concept would eliminate two required pedestrian crossings for those who want to walk between the Mission and the Museum of Natural History. Pedestrians could still use the existing crosswalk just north of the Mission Plaza to access the Rose Garden and Mission Historical Park on the east side of the corridor.





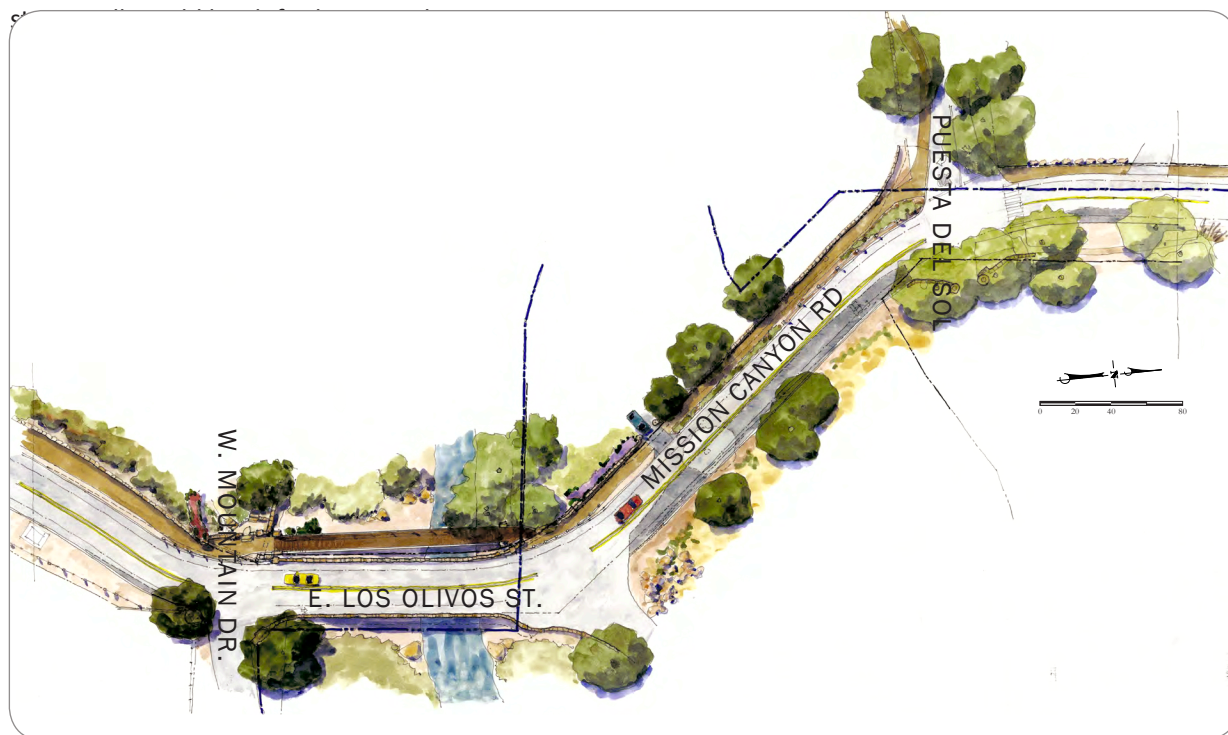
## 3.4 Segment #3: North of APS to North of Puesta del Sol

### SEGMENT #3

**East Los Olivos Street/Mission Canyon Road from north of APS to north of Puesta del Sol.**

The third segment in the corridor is within the City's and County's jurisdiction. East Los Olivos Street becomes Mission Canyon Road at West Mountain Drive and the bridge over Mission Creek. The concept for this segment continues the pedestrian pathway on the west side of the corridor and includes a new pedestrian bridge, road realignment, stone wall alterations, crosswalk relocation, and other changes to ensure continuous pedestrian access. This segment is key to meeting the goal of a continuous west side pathway and its feasibility relies on close coordination between the City and County, road realignments, and private property owner consent for easements and changes to stone walls.

Starting at the southern end of the segment and within the City's jurisdiction, there are two historic pillars on either side of East Los Olivos Street that once supported the elevated Mission aqueduct. These pillars form a pinch point where the road and pathway narrows significantly. To accommodate the west side pathway around one of the pillars, a portion of the road would be realigned to the east. The pillar would not be altered or relocated. Just past the pillar, there is a stone wall and gate for access into a garden at the Mount Calvary Monastery Retreat House (505 East Los Olivos Street). An approximately 30-foot segment of the





approximately 15 feet of a perpendicular stone wall would be removed to allow access to a new pedestrian bridge on the west side of the existing stone bridge. This proposal requires an easement from the private property owner.

The new pedestrian bridge would be adjacent to but separate from the existing historically significant stone bridge. Preliminary investigations indicate it may be possible to design a pedestrian bridge at this site so that large trees would not need to be removed. On the north side of the new pedestrian bridge, approximately 4 feet of the existing “stegosaurus” stone wall (on private property) would be shifted west to allow pedestrians to continue on the west side pathway. This proposal requires an easement from the private property owner. The west side pathway would continue parallel to the stegosaurus wall and turn west onto Puesta del Sol where pedestrians would have direct access to the Santa Barbara Museum of Natural History. A landscaped buffer would be included where there is enough room available.



CROSS-SECTION AT BRIDGE OVER MISSION CREEK



CROSS-SECTION AT MOUNTAIN DRIVE

Approximately 200 linear feet of the roadway would need to be realigned eastward to accommodate the west side pathway, between the entrance to Rocky Nook Park and Puesta del Sol. Several modifications are proposed to the frontage area of Rocky Nook Park to accommodate the concept: (1) the road centerline and northbound lane would be shifted approximately 14 feet east onto park property; (2) an existing mature sycamore tree and several smaller offshoots would be removed; and (3) utilities would be relocated including at least one electrical power pole and possibly a storm drain.

An area of about 700 square feet on Rocky Nook Park property would be needed from the existing ROW to new edge of pavement. This concept provides an opportunity to improve the Rocky Nook Park frontage with a new formalized road shoulder and new east side informal pedestrian pathway in the park through existing boulders and oak trees. This new pathway would connect to the existing pathway that leads to the fire station at Foothill Road. This proposal has received support from the County Parks Division.

Finally, the proposal includes relocating the crosswalk at Mission Canyon Road and Puesta del Sol to encourage pedestrian access from the west side to Rocky Nook Park. In coordination with the other proposed improvements, the crosswalk would be relocated from the north side of the intersection to the south side (not shown in the concept plan) to improve sight recognition between vehicles and

pedestrians. The road realignment and the crosswalk relocation would require consent and an easement from the Santa Barbara Woman's Club because a boulder and cobble pile at the Club's ingress driveway would need to be relocated to provide adequate clearance and access for pedestrians. The Club's stone pillars flanking the driveway would not be impacted.



CROSS-SECTION AT ROCKY NOOK PARK



## 3.5 Segments #4 and 5: Todos Santos Lane to Foothill Road

### SEGMENTS #4 and 5

#### Mission Canyon Road north of Puesta del Sol to Todos Santos Lane and Todos Santos Lane to Foothill Road.

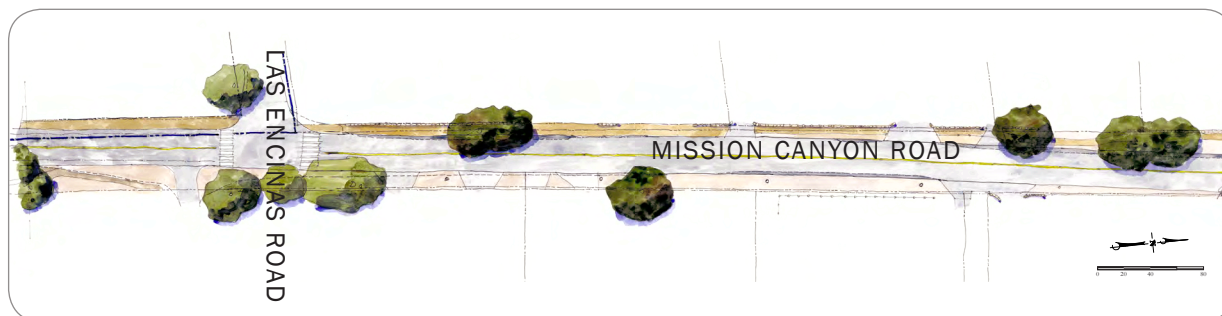
These segments on Mission Canyon Road are primarily within the County's jurisdiction (a 300-foot stretch on the west side of Mission Canyon Road between Puesta del Sol and Las Encinas Road is in the City). The concept for these segments continues the west side pathway north to Foothill Road.

Mission Canyon Road is straight and slopes uphill in these segments, with variable width and a semi-rural aesthetic of large trees, stone walls, hedges, and single family homes with large setbacks lining the road.

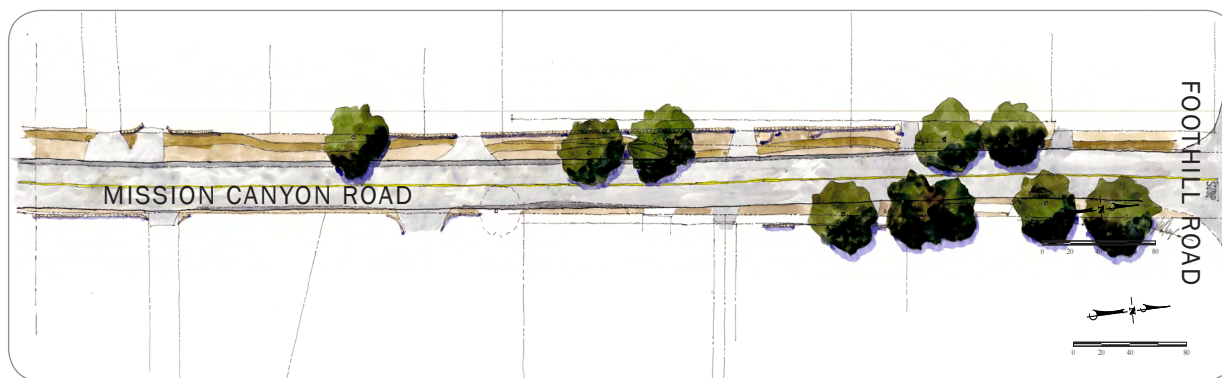
There is an existing unimproved west side pedestrian pathway that starts at Las Encinas Road and ends at Foothill Road. The enhancement for these segments is not extensive and includes road restriping and improvements in the existing ROW on the west side. Because the road width varies, there is not enough room for Class II bikelanes (4-foot width required) without making significant changes to the semi-rural character. Therefore, the concept restripes the road to create a wider bike area on the east side of the road where there is a significant uphill grade. The bike area would allow room for cars to pass the slower uphill cyclists. The downhill lane would be narrower because downhill cyclists are often traveling at or faster than the speed of cars. Although not yet depicted on the plan

illustrations, the roadway striping improvements also allow the concept plan to include a short left-turn pocket for northbound vehicles turning into Las Encinas Road. In addition, the two existing crosswalks across Mission Canyon Road would be consolidated into one at the south side of Las Encinas Road. The north crosswalk would be eliminated.

Other improvements for bicyclists and vehicles are proposed, such as grinding down the asphalt crown that has built up over many years of pavement laying and filling in the eroded road shoulder with pavement material to reduce the shoulder drop off that currently exists. The existing pathway would be improved for ADA accessibility and consistency with the rest of the corridor and sight distances from private roads and driveways improved where necessary.



SEGMENT #4 - NORTH OF PUESTA DEL SOL TO TODOS SANTOS LANE



SEGMENT #5 - TODOS SANTOS LANE TO FOOTHILL ROAD

## 3.6 Long Term Concepts

### INTRODUCTION

Based on community input at the first public workshop, the project team compiled proposals that did not meet the immediate goal of a continuous west side pedestrian pathway and were not feasible in the short term. These proposals were not included in the concept plans because they involved complex planning issues and divergent public opinions. Addressing these proposals would require time and resources beyond those available under the current grant and scope of work. At the second public workshop, the community was asked to vote on whether these additional concepts should be included in a long term plan. They are presented here in the order of preference (i.e., most desired to least desired) stated by the community.

### UNDERGROUND UTILITIES

#### COMMUNITY COMMENTS

- **Utility pole at East Los Olivos Street and Laguna Street**

Utility pole at East Los Olivos Street and Laguna Street should be undergrounded, it is unsightly for views of the Old Mission.

- **Above Ground Utilities Unsafe**

All above ground utility lines are unsafe and not in code.

Utility poles are frequently hit by cars and unsightly for views.

### CONCEPT PLAN PROPOSAL

The utility pole on the east side of Mission Canyon Road adjacent to Rocky Nook Park is proposed to be removed and/or relocated.

### LONG TERM PLAN

A comprehensive utility undergrounding program for improved safety and aesthetics should be considered for the long term plan, subject to cost and prioritization with other areas of the City and County that are planning for utility undergrounding. The California Public Utilities Commission (CPUC) sets policies and procedures for conversion of overhead power lines to underground facilities, called "Rule 20." Under Rule 20, undergrounding projects are financed by utility rate funds, combined rate funds and local tax proceeds, depending on various provisions. To qualify for this funding, the project must produce a benefit to the general public by satisfying certain criteria.

The electrical company, in conjunction with a City or County appointed Utility Undergrounding Program Committee, acts on requests to place lines underground. The Committee makes recommendations to the County Board of Supervisors or City Council who approve the project and establishment of an undergrounding district by resolution.





## IMPROVE ALAMEDA PADRE SERRA (APS) INTERSECTION

### COMMUNITY COMMENTS

- Difficult left-hand turn from APS onto Los Olivos Street.
- The east side sidewalk ends at APS and is unsafe.
- Need continuous safe walking from East Los Olivos Street to Puerta del Sol Road.
- Landscape blocks visibility at APS triangle.
- Drainage down APS causes erosion in Mission Historical Park.
- Reconsider the location of the crosswalk from the east side of the Mission to the Mission Historical Park.

### CONCEPT PLAN PROPOSAL

No changes are planned to the APS intersection.

### LONG TERM PLAN

Reconstruction of the intersection into a roundabout or “T”-intersection could be considered to provide improved vehicle turning movements, pedestrian crossing, drainage, and landscape enhancements.

These improvements were not included in the concept plans because they involved complex planning issues and divergent public opinions. Addressing these improvements would require time and resources beyond those available under the

current grant and scope of work. The proposal would be within the City’s jurisdiction and subject to City Council funding and approval.



## RECONSTRUCT MOUNTAIN DRIVE INTERSECTION OR ALTER TRAVEL DIRECTION

### COMMUNITY COMMENTS

- Limited visibility at Mountain Drive and Mission Canyon Road intersection.
- The left turn up to Mountain Drive from Mission Canyon Road is difficult during peak hours.
- Crossing Mountain Drive is dangerous for pedestrians and there is no defined path.
- The eastside path at Mountain Drive should be improved/repared.
- Consider one-way traffic up Mountain Drive.

### CONCEPT PLAN PROPOSAL

No changes are proposed to the Mountain Drive intersection. The road realignment required for the west side pathway would further narrow the existing pathway at the south end of the intersection.

### LONG TERM PLAN

Consider reconstructing the intersection to improve visibility or add turn lanes. These improvements were not included in the concept plans because they are not necessary to meet the goal of a continuous west side pathway. The proposal would be within the City's jurisdiction and subject to City Council funding and approval.



MOUNTAIN DRIVE INTERSECTION

## INSTALL RAPID FLASHING BEACONS AT PUESTA DEL SOL AND LAS ENCINAS ROAD

### COMMUNITY COMMENTS

- Improve safety for pedestrians.
- Pedestrians are not visible using the crosswalk at Las Encinas Road, particularly when cars pass on the right at Las Encinas Road.

### CONCEPT PLAN PROPOSAL

Crosswalk relocations at the intersections of Mission Canyon Road and Puesta del Sol and Mission Canyon Road and Las Encinas Road are proposed to increase pedestrian visibility and reduce conflicts with traffic.

### LONG TERM PLAN

Consider adding rapid flashing beacons at crosswalks. These were not included in the concept plans because they could conflict with community desires to retain the rural and historic quality of the corridor. They could be installed in a later phase after the crosswalk relocations are made if considered desirable.





## RECONFIGURE INTERSECTION AT LAGUNA AND LOS OLIVOS

### COMMUNITY COMMENTS

- It takes too long for pedestrians to cross the intersection and visibility is challenging. The intersection should be squared and the crosswalk configuration improved at Laguna Street and Los Olivos Street.

### CONCEPT PLAN PROPOSAL

No changes are proposed to the intersection of Laguna Street and East Los Olivos Street.

### LONG TERM PLAN

Consider reconfiguring the intersection of Laguna Street and East Los Olivos Street to shorten pedestrian crossings and improve vehicle movement. This proposal was not included in the concept plans because it involved potential conflicts with historic resources and divergent public opinions. Addressing this proposal would require time and resources beyond those available under the current grant and scope of work. This proposal would be within the City's jurisdiction and subject to City Council funding and approval.



INTERSECTION AT EAST LOS OLIVOS STREET  
AND LAGUNA STREET



LONG TERM CONCEPTS  
COMMUNITY RECOMMENDED CONCEPT PLANS



## INCREASE VISIBILITY AT PRIVATE DRIVES AND DRIVEWAYS

### COMMUNITY COMMENTS

- There is limited visibility for cars exiting Todos Santos and Glendessary lanes onto Mission Canyon Road due to road cross slope and visual obstructions.
- Improve issues with egress from driveways.

### CONCEPT PLAN PROPOSAL

Implementation of the concept plans would increase visibility at some private drives and driveways as follows:

- Trimming landscaping and overhanging trees in the ROW would incrementally increase visibility.
- Driveway slopes would be reduced with the new pathways to improve ADA accessibility. Egress from the private driveway at the Kay property (609 Mission Canyon Road) would be improved with the road realignment.
- The Mission Canyon Road cross slope “crown” that has built up over time from the application of asphalt would be reduced, resulting in improved visibility from Todos Santos and Glendessary lanes.

### LONG TERM PLAN

Other items that could improve visibility include removing trees, major re-grading, and adjusting or moving stone walls and boulder piles. These measures would likely not be supported by the community due to impacts to community character and were not to be considered over the long term.



TODOS SANTOS LANE AT MISSION CANYON ROAD



WEST SIDE OF MISSION CANYON ROAD

## CREATE CONTINUOUS EASTSIDE PATHWAY

### COMMUNITY COMMENTS

- Improve walkability on both sides of Mission Canyon Road and allow safe walking across the stone bridge.
- There is a lack of pedestrian access on the upper east side of the corridor.
- The existing east side pathway is unsafe and unwelcoming with mixed pathway materials such as gravel and concrete.
- Physical impediments need to be removed from existing east side pathway.

### CONCEPT PLAN PROPOSAL

The concept plans promote a continuous west side pathway from Laguna Street to Foothill Road. No changes are proposed to the existing east side pathways or the east side pedestrian walkway attached to the bridge crossing Mission Creek. Access from the west side of the corridor to Rocky Nook Park (on the east side) is important and improved by moving the existing crosswalk at Mission Canyon Road and Puerta del Sol from the northwest corner of the intersection to the southwest corner. This change will improve sight recognition between vehicles and pedestrians.



MOUNTAIN DRIVE CONSTRAINED BY HISTORIC RESERVOIR



NO PATH ON EAST SIDE OF MISSION CANYON ROAD

### LONG TERM PLAN

Due to limited space in the ROW, improving walkability on both sides of Mission Canyon Road would require new ROW, significant vegetation removal, and potential significant impacts to historic resources. Pursuit of a continuous east side pathway would likely negate the ability to provide a continuous west side pathway.

The long term plan could consider other ways to improve access on the east side in concert with improvements to the APS and Mountain Drive intersections. In the long term, the existing east side pedestrian walkway attached to the bridge over Mission Creek could be replaced with a new pedestrian bridge created on the existing sewer pipe to the east of the bridge. The existing deteriorating east side pathway from Laguna Street to APS could be replaced with better materials and better access could be created from Mission Historical Park to the APS sidewalk. The Mission Heritage Trail Association is working on other visionary concepts to improve the east side of the corridor in the long term.

## INCREASE STREET LIGHTING IN THE MISSION CANYON CORRIDOR

### COMMUNITY COMMENTS

- Existing lighting is inadequate.
- Street light glare detracts from semi-rural character.

### CONCEPT PLAN PROPOSAL

No changes to street lighting are proposed in the concept plans.

### LONG TERM PLAN

There was minimal community support for increased street lighting. Preservation of the dark night sky is desired in the semi-rural Mission Canyon Plan area. If pathway lighting is desired in the future, it should be minimal and fully shielded to achieve desired illumination at ground level.



UTILITY POLE AND LIGHT FIXTURE AT BRIDGE



## WIDEN BRIDGE OVER MISSION CREEK

### COMMUNITY COMMENTS

- Widen the stone bridge.
- Add a bike lane to the stone bridge.
- Retrofit the stone bridge for seismicity but maintain its character.
- Eliminate graffiti on the historic stone bridge.
- Address sewer line in relation to the historic bridge.

### CONCEPT PLAN PROPOSAL

The concept plans do not include any changes to the stone bridge. The existing east side pedestrian walkway would remain in place. Future bridge rehabilitation is programmed by the City of Santa Barbara. The existing width already accommodates bike lanes and there is a program for ongoing graffiti removal.

### LONG TERM PLAN

There is little or no community support for widening the bridge. For improved aesthetics and access to Rocky Nook Park, the long term plan could consider relocating the sewer line and the existing pedestrian walkway attached to the bridge, perhaps combining the pedestrian walkway with the sewer pipe crossing.



## 3.7 Other Options

The concept of a continual path on the west side of the corridor, particularly between the Mission and museum, was an obvious solution to pedestrian safety and circulation issues because it would avoid crossing an extremely busy road and two difficult intersections. However, due to concerns about potential impacts to historic stone walls, other options were considered for inclusion in the proposed concept plans for a continuous west side pathway. These options were eventually rejected for inclusion for the reasons detailed below.

### OPTION #1

#### **West side pathway to the museum with an alternative crossing location over Mission Creek.**

This option would potentially eliminate the need to move and/or create openings in the stone walls south and north of the existing bridge over Mission Creek. Two potential ways to accommodate this were evaluated:

1. Gain access via an existing private road and through several private properties to a new pedestrian bridge over Mission Creek to the museum; or
2. Gain access through an existing gate and through private property to a new pedestrian bridge over Mission Creek.

These options were rejected for the following reasons.

- They would require much larger and additional easements through private property than the proposed concept plans, including property owned by the Mt. Calvary Monastery, Old Mission Santa Barbara, San Roque School Charitable Trust, and museum.
- They would limit general public accessibility through the corridor because the path would terminate onto private property owned by the museum which has limited hours (10:00 am to 5:00 pm), is closed on at least four holidays, charges admission for non-members, and admits visitors through a central office located adjacent to Puesta del Sol.
- The south side of the creek is much higher than the north and slopes are very steep. For ADA accessibility, the path would have to be thousands of feet longer than the proposed concept plans. Also, the existing gate is not wide enough for ADA accessibility.

### OPTION #2

#### **Improve the east side pathway between Mission Historical Park and Rocky Nook Park.**

Most users of the corridor agree that measures could be taken to improve the existing east side pedestrian pathway and there are many valuable features to visit on the east side for residents and visitors. Existing problems include poor condition of pathways, constraints and missing pathway segments that force pedestrians into the road, no crosswalks and poor visibility at intersections with APS and Mountain Drive, and lack of accessibility for people with disabilities. Furthermore, pedestrians have to cross East Los Olivos Street/Mission Canyon Road twice to walk between the Mission and the museum.

Improved access on the east side is not being considered at this time is due to potential impacts to historic resources and physical constraints that are detailed in Chapter 2.5. Furthermore, changes to the APS and Mountain Drive intersections could be controversial and would expand the scope of this project significantly. As noted earlier in this chapter, it is recommended that improvements to portions of the east side of the corridor be considered in a long term plan.

# 4

## Costs and Potential Funding Sources





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# 4.1 Costs and Potential Funding Sources

## COST ESTIMATES

City and County engineers have prepared preliminary estimates for the concept plans proposed in this report, i.e., the continuous west side pedestrian pathway, and improvements for pedestrian, bicyclist, and vehicle safety. Segments #1 and #2 and most of segment #3 are located within the City of Santa Barbara and segments #4 and #5 are located within the County. All of segment #3 and a portion of segment #4, as far north as Las Encinas Road, may be annexed into the City in the near future as part of a separate project that will go before City decision makers.

City engineers preliminarily estimate the cost to fully construct the concept plans from East Los Olivos Street to Las Encinas Road to be approximately \$2.74 million. The estimate includes 30% for administration, environmental review, special studies, design services and construction management, along with a 35% construction cost contingency to accommodate time necessary to secure construction funding and uncertainties in the final scope of construction work. County engineers estimate the cost to fully construct the concept plans from Las Encinas Road to Foothill Road to be approximately \$600,000. No estimates have been made for the long-term concepts presented in Chapter 3.6 because they are not included in the concept plans.

## POTENTIAL FUNDING SOURCES

This section identifies and summarizes potential funding sources to implement the concept plans.

Federal, state, and local government agencies invest billions of dollars every year in the nation's transportation system. Only a fraction of that funding is reserved to plan and improve conditions for pedestrians and bicyclists. Even though funds are limited, they are available. The list below contains current funding sources. In the future it is expected that new federal and/or state legislation may be passed modifying or eliminating certain sources of funding. County and City staff will work closely with their colleagues at local, state, and federal agencies to monitor these changes and obtain funding to implement the concept plan.

## FEDERAL SOURCES

### TRANSPORTATION ALTERNATIVES PROGRAM

Transportation Alternatives Program (TAP) is a new funding source under MAP-21 that consolidates three formerly separate programs under SAFETEA-LU: Transportation Enhancements, Safe Routes to School, and the Recreational Trails Program. These funds may be used for a variety of pedestrian, bicycle, and streetscape projects, including sidewalks, bikeways, and multi-use paths. Eligible activities under the TAP include:

1. Transportation Alternatives as defined by Section 1103(a)(29). This category includes the construction, planning, and design of a range of bicycle and pedestrian infrastructure including, "on-road and off-road trail facilities for pedestrians, bicyclists, and other active forms of transportation, including sidewalks, bicycle infrastructure, pedestrian and bicycle signals, traffic calming techniques, lighting

and other safety-related infrastructure, and transportation projects to achieve compliance with the Americans with Disabilities Act (ADA) of 1990." Infrastructure projects and systems that provide "Safe Routes for Non-Drivers" are new eligible activities.

2. Safe Routes to School (SR2S). SR2S programs are intended to increase the number of children walking and bicycling to school by making it safer for them to do so. SR2S-funded projects must be "in the vicinity" of a school (K-12). Eligible projects may include:
  - Engineering improvements. These physical improvements are designed to reduce potential bicycle and pedestrian conflicts with motor vehicles. Physical improvements may also reduce motor vehicle traffic volumes around schools, establish safer and more accessible crossings, or construct walkways, trails, or bikeways.
  - Education and Encouragement Efforts. These programs are designed to teach children safe bicycling and walking skills while educating them about the associated health benefits and environmental impacts.
  - Enforcement Efforts. These programs aim to ensure that traffic laws near schools are obeyed. Law enforcement activities apply to cyclists, pedestrians, and motor vehicles alike.

### **SURFACE TRANSPORTATION PROGRAM**

The Surface Transportation Program (STP) provides states with flexible funds which may be used for a variety of highway, road, bridge, and transit projects. A wide variety of bicycle and pedestrian improvements are eligible, including on-street bicycle facilities, off-street trails, sidewalks, crosswalks, bicycle and pedestrian signals, parking, and other ancillary facilities. Modification of sidewalks to comply with the requirements of the ADA is also an eligible activity. Unlike most highway projects, STP-funded bicycle and pedestrian facilities may be located on local and collector roads.

### **HIGHWAY SAFETY IMPROVEMENT PROGRAM (HSIP)**

HSIP provides \$2.4 billion nationally for projects and programs that help communities achieve significant reductions in traffic fatalities and serious injuries on all public roads, bikeways, and walkways. HSIP is a data-driven funding program, and eligible projects must be identified through analysis of crash experience, crash potential, crash rate, or other similar metrics. Infrastructure and non-infrastructure projects are eligible for HSIP funds. Bicycle and pedestrian safety improvements, enforcement activities, traffic calming projects, and crossing treatments for active transportation users in school zones are examples of eligible projects.

### **STATE SOURCES**

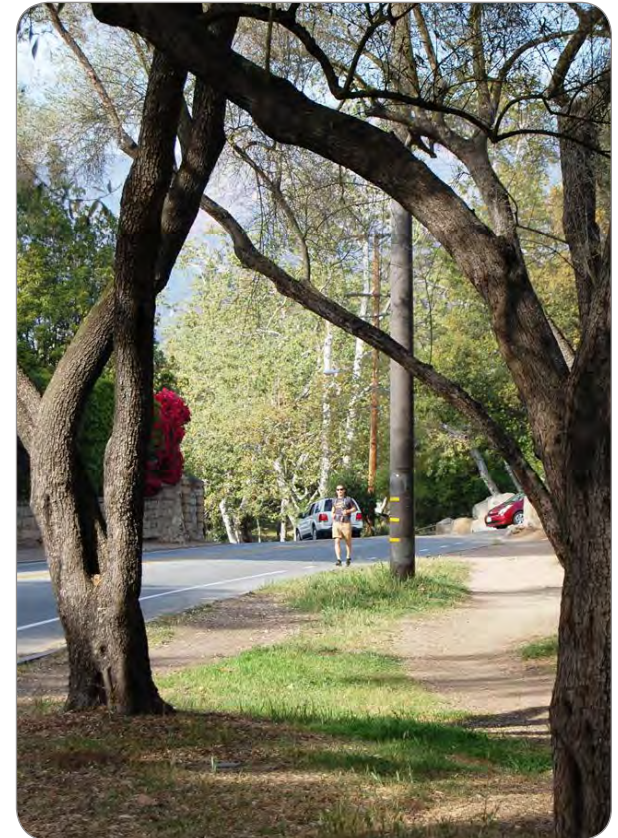
#### **ACTIVE TRANSPORTATION PROGRAM**

The Active Transportation Program (ATP) consolidates the federal programs, Bicycle

Transportation Account, Safe Routes to Schools Program, and Recreational Trails Program. The ATP's authorizing legislation also includes placeholder language to allow the ATP to receive funding from the newly established Cap-and-Trade Program in the future.

The California Transportation Commission writes guidelines and allocates funds for the ATP, while the ATP will be administered by the Caltrans Division of Local Assistance. Goals of the ATP are currently defined as the following:

1. Increasing the proportion of trips accomplished by biking and walking.
2. Increasing safety and mobility for non-motorized users.
3. Advancing active transportation efforts of regional agencies to achieve the greenhouse gas reduction goals.
4. Enhancing public health.
5. Ensuring that disadvantaged communities fully share in the benefit of the program.
6. Providing a broad spectrum of projects to benefit many types of active transportation users.



EAST SIDE PATH APPROACHING MOUNTAIN DRIVE



## REGIONAL AND LOCAL SOURCES

### SANTA BARBARA COUNTY ASSOCIATION OF GOVERNMENTS (SBCAG)

The following programs, available through SBCAG, provide potential funding sources for parking and transportation projects.

1. Surface Transportation Program (STP). STP-eligible projects in Mission Canyon Corridor include the repair or rehabilitation of roadways in the form of paved shoulders, bike lanes, and repaving to include crosswalks. SBCAG's annual budget for STP is \$4 million.
2. Measure A Program Local Street and Transportation Improvement (LSTI) Funding. Measure A funds can be used to repair local roads. SBCAG distributes approximately \$20 million per year through this program to local jurisdictions. The most applicable uses for these funds are the repair or rehabilitation of roadways in the form of paved shoulders, bike lanes, and repaving to include crosswalks.
3. Measure A South County Safe Routes to School, and Bicycle and Pedestrian Program. The Measure A Program includes funding in the South County for a bicycle and pedestrian and safe routes to school program. In the South County, \$13 million is available for the 30-year period for the bicycle and pedestrian program, and an additional \$13 million is available for the safe routes to school program. The Investment Plan requires the allocation of funding to be

conducted through a competitive grant process. SBCAG conducts a call for projects every three years for the South County program.

### DEVELOPMENT IMPACT MITIGATION FEES

Development impact mitigation fees are a potential source of funds for providing some transportation improvements. A local government agency may assess fees depending upon the details of a proposed project and the proposed improvements.

### ROADWAY CONSTRUCTION, REPAIR AND UPGRADE

Future road widening and construction projects are one means of providing improved pedestrian and bicycle facilities. To ensure that roadway construction projects provide these facilities where needed, it is important that the review process includes input pertaining to consistency with the proposed system. In addition, California's 2008 Complete Streets Act and Caltrans' Deputy Directive 64 require that the needs of all roadway users be considered during "all phases of state highway projects, from planning to construction to maintenance and repair."

### UTILITY INSTALLATION PROJECTS

Cable TV and telephone companies sometimes need new cable routes within the public ROW. Recently, this has most commonly occurred during expansion of fiber optic networks. Since these projects require a significant amount of advance planning and disruption of curb lanes, it may be possible to request reimbursement for affected sidewalks and streets to mitigate construction impacts.

In cases where cable routes cross undeveloped areas, it may be possible to provide for new sidewalks or bikeway facilities following completion of the cable trenching.

### OTHER SOURCES

Local sales taxes, fees, and permits may be implemented as new funding sources for pedestrian and bicycle projects. However, any of these potential sources would require a local election. Volunteer programs may be developed to substantially reduce the cost of implementing some improvements, such as the "adoption" of street trees or the dedication of street furniture. A local construction company may donate or discount services.



BUSY DAY ON EAST LOS OLIVOS STREET

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# 5

## Phasing Plan and Next Steps





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# 5.1 Phasing Plan and Next Steps

Future improvements along the Mission Canyon corridor would occur in phases as funding becomes available. The improvements would provide for continuous pedestrian access on the west side of the corridor and improved bicyclist passage from the East Los Olivos Street/Laguna Street intersection in the City to the Mission Canyon Road/Foothill Road intersection in the County. This chapter provides a brief overview of the tasks necessary to move the concept plans forward for implementation.

## PHASING PLAN

Segments #1 and #2 are located in the City's jurisdiction. In addition, a significant portion of segment #3 is in the City. As a result, segment #3 will require close coordination between the City and County. Completion of these three segments is likely to take longer due to the greater costs involved, the need to continue working with affected private property owners and to obtain ROW, the need for more detailed engineering and aesthetic designs, especially for the new pedestrian bridge, and additional review and coordination with the City's Historic Landmarks Commission.

Segments #4 and #5 are located within the County's jurisdiction, and, as discussed in Chapter 3, the proposed enhancements along these segments would require comparatively less work to develop final design engineering plans. In addition, as the costs estimated for these improvements are relatively lower (Chapter 4.1) County Public Works anticipates that funding for some of these improvements may be obtained within a year or

two. Thus, segments #4 and #5 may be the initial phase of the project to move forward.

## NEXT STEPS

Implementation of the project will require several steps to move the concept plans forward to the point of construction. These next steps include, but are not limited to:

- Formal survey of the road right-of-way, where required.
- Preliminary design engineering.
- Selection of materials for each improvement.
- Environmental review pursuant to CEQA.
- Review by County and City committees, where needed.
- Permits and/or rights-of-way, where needed.
- Final design engineering.

The County and City will continue to coordinate work efforts seeking grants and other funding sources to complete planning and environmental review, and to construct the identified improvements.



MISSION CANYON ROAD NORTH OF  
BRIDGE AT ROCKY NOOK PARK

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# A

## Appendix A - Self Guided Tour Booklet



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# Appendix A - Self Guided Tour Booklet

## Self-Guided Tour for the Mission Park to Mission Canyon Multimodal Improvement Project

**This tour can be taken by vehicle, walking, or bicycling; however, walking is the preferred method in order to stop and take notes.**

The intent of providing this booklet is to provide a guide for interested citizens to use before or after attending any one of three planned public workshops beginning in Fall 2013.

The tour begins at the intersection of Laguna Street and East Los Olivos Street in the City of Santa Barbara and ends at the intersection of Mission Canyon Road and Foothill Road (State Route 192) in the County of Santa Barbara. The travel corridor has been broken into 6 segments. Each segment is accompanied by directions, brief narrative, a map, and space to take notes.

Several roads and intersections must be crossed to complete the tour, please be careful. Thank you for your time and enjoy your tour.

For additional information, please visit Santa Barbara County Planning and Development Department, Long Range Planning Division website at the address below or contact Rosie Dyste, Senior Planner, Long Range Planning Division, (805) 568-3532

[http://longrange.sbcountyplanning.org/planareas/mission\\_canyon/mc\\_multimodal.php](http://longrange.sbcountyplanning.org/planareas/mission_canyon/mc_multimodal.php)

This project is funded by a Caltrans Community Based Transportation Planning Grant.

### SEGMENT 1

The tour begins at the corner of Laguna and East Los Olivos Street in the City of Santa Barbara. Parking is available in the Mission Santa Barbara parking lot or along Laguna Street.

Proceed up East Los Olivos Street on the existing pedestrian path on either side of the East Los Olivos Street or use the existing Class II bike lane.



Notes for Segment 1

What features/issues stand out in this segment?

How could this segment be improved?



**SEGMENT****2**

Segment 2 begins at the existing crosswalk just north of Mission Santa Barbara. If you were walking on the Mission (west) side of the street, you should cross to the east side here. Proceed on the existing sidewalk along East Los Olivos Street. Carefully cross the road at APS, there is no crosswalk.



Notes for Segment 2

What features/issues stand out in this segment?

How could this segment be improved?

**SEGMENT****3**

Segment 3 begins at the olive grove just after the crossing of APS. Proceed on the dirt path along East Los Olivos Street until you reach Mountain Drive. You are still within the City of Santa Barbara.



Notes for Segment 3

What features/issues stand out in this segment?

How could this segment be improved?

**SEGMENT****4**

Segment 4 begins at Mountain Drive, crosses Mission Creek, and proceeds past Rocky Nook Park to the crosswalk to the Santa Barbara Museum of Natural History at Puesta del Sol. The bridge over Mission Creek is the transition from the City to the County and where Mission Canyon Road begins. Be careful crossing Mountain Drive and take the walkway attached to the bridge over Mission Creek.



Notes for Segment 4

What features/issues stand out in this segment?

How could this segment be improved?

**SEGMENT****5**

Segment 5 is from the crosswalk at Puesta del Sol to the crosswalk at Las Encinas Road. You will be traveling in front of Rocky Nook Park and the Santa Barbara Woman's Club on the east side of Mission Canyon Road.



Notes for Segment 5

What features/issues stand out in this segment?

How could this segment be improved?

**SEGMENT**  
**6**

Segment 6 requires crossing Mission Canyon Road at Las Encinas to use an existing footpath on the west side of Mission Canyon Road up to Foothill Road. The tour ends at Foothill Road.

**Notes for Segment 6**

What features/issues stand out in this segment?

How could this segment be improved?



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## ATTACHMENT B

### NOTICE OF EXEMPTION

**TO:** Santa Barbara County Clerk of the Board of Supervisors

**FROM:** Julie Harris, Planner, Planning and Development, Long Range Planning Division

The project or activity identified below is determined to be exempt from further environmental review requirements of the California Environmental Quality Act (CEQA) of 1970, as defined in the State and County Guidelines for the implementation of CEQA.

**APN:** Not applicable      **Case No.:** Not applicable

**Location:** A ¾-mile long, two-lane road starting at the intersection of Laguna Street and East Los Olivos Street in the City of Santa Barbara and ending at the intersection of Mission Canyon Road and Foothill Road in the unincorporated Mission Canyon area in the County of Santa Barbara.

**Project Title:** Mission Park to Mission Canyon Multimodal Improvements Plan

**Project Applicant:** Not applicable

**Project Description:** The project consists of the Mission Park to Mission Canyon Multimodal Improvements Plan (plan), a grant-funded partnership between the County of Santa Barbara (County) and the City of Santa Barbara (City) to prepare concept level plans for pedestrian, bicyclist, and motor vehicle improvements in this historic and highly scenic corridor.

No continuous sidewalk or pedestrian facility exists through this busy corridor. Although a few facilities exist at various points, they do not comply with the American with Disabilities Act (ADA). The lack of continuous facilities makes walking and biking in the area difficult and hazardous for residents and visitors.

The concept plans for multimodal improvements along the plan corridor include the following:

- Continuous pedestrian path on the west side of the corridor. The path must comply with the ADA and be consistent with the historical setting.
- Landscape buffers between path and roadway, where feasible.
- New detached pedestrian bridge parallel and west of the existing stone bridge over Mission Creek.
- Bike lanes extending north to Puerta del Sol transitioning into bike routes in the County's jurisdiction where the road narrows.
- Relocation of several crosswalks.
- Left-turn pocket from Mission Canyon Road to Las Encinas Lane.

The project also includes adoption of a Board of Supervisors resolution to direct County staff to work collaboratively with City of Santa Barbara staff to pursue funding opportunities to implement the plan.

**Name of Public Agency Approving Project:** Santa Barbara County (The County will receive and refer the plan to the Public Works Department. It will not approve or fund the improvements at this time.)

**Name of Person or Agency Carrying Out Project:** Santa Barbara County and City of Santa Barbara

**Exempt Status:** (Check one)

<input type="checkbox"/>	Ministerial
<input checked="" type="checkbox"/>	Statutory Exemption
<input type="checkbox"/>	Categorical Exemption
<input type="checkbox"/>	Emergency Project
<input checked="" type="checkbox"/>	General Rule, Section 15061(b)(3)

**Cite specific CEQA and/or CEQA Guideline Section:** Section 15262 Feasibility and Planning Studies for receipt and referral of the plan and Section 15061(b)(3) for adoption of a resolution.

**Reasons to support exemption findings:** A statutory exemption pursuant to Section 15262 states that a project involving only feasibility or planning studies for possible future actions which the agency has not approved or funded does not required preparation of an EIR or Negative Declaration but does require consideration of environmental factors. Consistent with this exemption, the plan is limited to preparation of concept plans for possible future actions which Santa Barbara County and the City of Santa Barbara have not approved, adopted, or funded. The plan recognizes various environmental factors in the area, such as aesthetic, historic, and biological resources. The concept plans were developed to avoid or minimize potential impacts on the environment. Future work to implement the plan may require an initial study to identify potentially significant effects on the environment. A negative declaration or environmental impact report may also be required along with consideration of designs or alternatives that would avoid or minimize any potential effects on the environment.

The project is the first step toward implementing one component of the County's recently adopted Mission Canyon Community Plan and policies within the City's Circulation Element and Pedestrian Master Plan. The plan does not have a legally binding effect on later activities. The plan was funded by grants that limit the project to concept plans only and do not allow the preparation of design engineered plans. Therefore, the plan qualifies for this exemption.

Section 15061(b)(3) states that a project is exempt from CEQA if the activity is covered by the general rule that CEQA applies only to projects which have the potential for causing a significant effect on the environment. Where it can be seen with certainty that there is no possibility that the activity in question may have a significant effect on the environment, the activity is not subject to CEQA. The adoption of a resolution by the Board of Supervisors directing County staff to work collaboratively with City staff to pursue funding opportunities to implement plan has no possibility of creating an effect on the environment because the resolution only directs County and City staffs to work collaboratively to pursue funding. Adoption of the resolution does not commit the County to later activities or any specific project. Should funding be pursued and obtained, site specific studies would be required to develop detailed designs beyond the conceptual level, which would at that time require complete environmental review pursuant to CEQA.



**Lead Agency Contact Person:** Julie Harris

**Phone #:** (805) 568-3518

**Department/Division Representative:** \_\_\_\_\_

**Date:** \_\_\_\_\_

**Acceptance Date:** \_\_\_\_\_

**Distribution:** Hearing Support Staff

**Date Filed by County Clerk:** \_\_\_\_\_

## ATTACHMENT C

### COUNTY AND CITY POLICIES

County of Santa Barbara Mission Canyon Community Plan:

***Action CIRC-MC-2.2:*** *The County shall actively pursue siting a pedestrian on-road trail adjacent to Mission Canyon Road from the intersection with Mountain Drive to the Santa Barbara Botanic Garden. Trail design and siting shall be consistent with the semi-rural neighborhood character along Mission Canyon Road.*

***Policy CIRC-MC-3:*** *The County shall coordinate with the City of Santa Barbara to seek grants and other funding sources to design and implement a plan to improve multimodal access and safety for residents and visitors traveling between Mission Santa Barbara, Rocky Nook Park, Santa Barbara Museum of Natural History, and local schools and neighborhoods within and adjacent to the City/County boundary at Mission Canyon Road, in accordance with the streetscape plan for the Scenic Corridor (see Action VIS-MC-3.2).*

*Improvement plans for this roadway shall include, but not be limited to, an evaluation of the following issue areas:*

- *Traffic patterns and on-street parking generated by special events at Mission Santa Barbara, the Santa Barbara Museum of Natural History, and the Santa Barbara Woman's Club;*
- *Unpermitted encroachments into the public road ROW;*
- *Dual purpose pedestrian trails/emergency vehicle turnout zones in appropriate locations, including the southbound segment of Mission Canyon Road and Los Olivos Street for use in an emergency evacuation with the aid of traffic control;*
- *Improvements to the Mission Creek bridge; and*
- *Historical resources evaluation by the City and County Historic Landmarks Commission/Committee.*

***Action CIRC-MC-3.1:*** *The County shall collaborate with the community and City of Santa Barbara to improve multimodal connections between Mission Santa Barbara, Rocky Nook Park, Santa Barbara Museum of Natural History, and local schools and neighborhoods adjacent to the City/County boundary at lower Mission Canyon Road and Los Olivos Street, consistent with the historical and scenic nature of this area.*

***Policy VIS-MC-3.2:*** *The County shall coordinate with the City of Santa Barbara, area nonprofit institutions, and residents to seek grants and other funding sources to design and implement the Phase II streetscape plan for the Mission Canyon Scenic Corridor, coordinated with multimodal access and safety improvement plans (see Policy CIRC-MC-3).*

*Aesthetic considerations for the streetscape plan could include the following programs:*

- *Design and materials for paths, crosswalks, and streetscape features;*

- *Protection of existing stone walls and other historic features;*
- *A signage plan;*
- *Landscaping recommendations; and*
- *Undergrounding utilities.*

City of Santa Barbara Circulation Element:

***Policy 5.1.2:*** *[in part] Identify and link major activity centers and destinations with walkways. This will consist of the following:*

- *Improving pedestrian access in and around the Mission by providing safe and attractive walking connections between the Mission, Rose Garden, Rocky Nook Park, Natural History Museum, and Alameda Padre Serra.*

City of Santa Barbara Pedestrian Master Plan:

***Policy 1.1:*** *The City shall expand the sidewalk network to increase walking for transportation and recreation.*

***Policy 1.2:*** *The City shall improve pedestrian safety and comfort at intersections.*

***Policy 1.3:*** *The City shall enhance pedestrian corridors.*



## COUNTY OF SANTA BARBARA



**SOUTH  
BOARD OF ARCHITECTURAL REVIEW  
APPROVED MINUTES  
Meeting of July 11, 2014**

Santa Barbara County  
Planning Commission Hearing Room  
Engineering Building, Room 17  
123 East Anapamu Street  
Santa Barbara, CA 93101  
(805) 568-2000

The regular meeting of the Santa Barbara County Board of Architectural Review Committee was called to order by the Chair, Alex Pujo, at 9:15 A.M., in the Santa Barbara County Engineering Building, Room 17, 123 East Anapamu Street, Santa Barbara, California.

**COMMITTEE MEMBERS PRESENT:**

Alex Pujo	Chair
Laurie Romano	Vice Chair
Valerie Froscher	
Jeff Yardy	
Dylan Chappell	
John Vrtiak	
Anita Hodosy-McFaul	SBAR Secretary
Anne Almy	Supervising Planner

**COMMITTEE MEMBERS ABSENT: Pamela Ettinger****REPORTERS:** None.**NUMBER OF INTERESTED PERSONS:** None.**ADMINISTRATIVE AGENDA:****I. PUBLIC COMMENT:** None.

**II. AGENDA STATUS REPORT:** Romano moved, seconded by Chappell and carried by a vote of 4 to 0 (Froscher, Yardy and Ettinger absent) to:

- Continue Item #8 14BAR-00000-00050 Oak Creek Company Lot 10 to the meeting of July 25, 2014 at the request of the applicant.
- Continue Item #8 14BAR-00000-00071 Oak Creek Company Lot 15 to the meeting of July 25, 2014 at the request of the applicant.

**III. MINUTES:** Romano moved, seconded by Vrtiak and carried by a vote of 5 to 0 to 1 (Chappell abstained, Ettinger absent) to approve the Minutes of June 20, 2014 as amended.

**IV. CONSENT AGENDA:**

<b>C-1. 14BAR-00000-00119</b>	<b>Wamdara, Inc.</b>	<b>Toro Canyon</b>
<b>Residence Additions and, Exterior Renovation</b>		
<b>14LUP-00000-00228 (Tammy Weber, Planner)</b>		
<b>Jurisdiction: Toro</b>		

Request of Barry Winnick, architect for the owner, Wamdara, Inc. c/o Alexander Radosevic, to consider Case No. 14BAR-00000-00119 for preliminary and final approval on consent of a residence office addition of approximately 396 square feet, guest bath expansion of approximately 220 square feet and relocation of doors and chimney. The following structures currently exist on the parcel: a residence and attached garage of approximately 11,406 square feet, guest house of approximately 800 square feet and pool cabana of approximately 789 square feet. The proposed project will require 633 cubic yards of cut and 190 cubic yards of fill. The property is a 4.74 acre parcel zoned 10-E-1 and shown as Assessor's Parcel Number 155-250-020, located at 815 Cima del Mundo in the Toro Canyon area, First Supervisorial District. (Continued from 6/20/14)

**ACTION:** Romano moved, seconded by Chappell and carried by a vote of 4 to 0 (Froscher, Yardy and Ettinger absent) to grant preliminary and final approval on consent of 14BAR-00000-00119.

**V. SBAR MEMBERS INFORMATIONAL BRIEFINGS:** None.

**VI. STAFF UPDATE:** None.

**VII. STANDARD AGENDA:**

**1. Courtesy Review Mission Canyon Corridor Conceptual Improvement Plan Mission Canyon**  
(Rosie Dyste, Planner) **Jurisdiction: Mission Canyon**

Request of Rosie Dyste, Senior Planner, Santa Barbara County Planning and Development, Long Range Planning Division, for courtesy review of conceptual plans for the Mission Park to Mission Canyon Multimodal Improvements project (Mission Canyon corridor) in the Mission Canyon Plan area, First Supervisorial District and City of Santa Barbara, First and Second Supervisorial Districts. The project begins at the intersection of Laguna and E. Los Olivos Street in the City of Santa Barbara and extends up Mission Canyon Road to the intersection of Mission Canyon Road and Foothill Road (State Route 192) in the County. The conceptual plans include a continuous pedestrian walkway on the west side of the corridor, a new pedestrian bridge, and bicycle lanes/shared vehicle/bicycle travel lanes.

Public Comments: Fred Sweeny, Lanny Ebenstein, Kellam de Forrest

**COMMENTS:**

- Landscaping in area between pedestrian path and bikeway should be natural and combined with bioswale.
- One Board member indicated that he would prefer to see a full Class II bike lane through the entire corridor, even if it means impacts to oak trees.
- Appropriate signage could help to distinguish the corridor and raise its prominence.
- This is an important project and it is going in the right direction.
- Low path lighting is appropriate, though could possibly incorporate tree lighting similar to the Santa Barbara Bowl.
- Consider a uniform travel speed and lane widths through the corridor.

Project received a courtesy review only, no action was taken.

**2. 14BAR-00000-00105 Canfield Residence Addition Santa Barbara**  
14LUP-00000-00171 (Kimberley McCarthy, Planner) **Jurisdiction: Goleta**

Request of Kevin Moore, architect for the owner, Taylor Canfield, to consider Case No. 14BAR-00000-00105 for preliminary approval of a residence addition of approximately 546 square feet and partial removal of garage of approximately 30 square feet. The following structures currently exist on the parcel: a residence of approximately 1,300 square feet and garage of approximately 425 square feet. The proposed project will require 10 cubic yards of cut and no fill. The property is a 10,000 square foot parcel zoned 10-R-1 and shown as Assessor's Parcel Number 067-181-024, located at 454 Vaquero Lane in the Santa Barbara area, Second Supervisorial District. (Continued from 6/06/14)

**COMMENTS:**

- Architecture is acceptable
- Increase ground cover and shrubs
- Add succulents into the pea gravel area
- Tie the landscape into the modern character of the architecture
- Need quantities and sizes on the landscape plan, along with specific information on irrigation, for Final Approval
- Return for Final on consent with the emphasis on landscaping

**ACTION:** Chappell moved, seconded by Romano and carried by a vote of 6 to 0 (Ettinger absent) to grant preliminary approval of 14BAR-00000-00105. Applicant may return for final approval on consent.



## COUNTY OF SANTA BARBARA

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### **HISTORIC LANDMARKS ADVISORY COMMISSION APPROVED MINUTES**

**Hearing of September 8, 2014  
Meeting Time: 11:00 a.m.**

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The regular hearing of the Santa Barbara County Historic Landmarks Advisory Commission was called to order by Chair Bob Cunningham, at 11:00 a.m., in the County of Santa Barbara Planning Commission Hearing Room, Engineering Building, Room 17, 123 East Anapamu Street, Santa Barbara, California.

#### **COMMISSIONERS PRESENT:**

RONALD NYE  
EDWIN LENVIK  
CARLA TOMSON  
BOB CUNNINGHAM  
BARBARA GREENLEAF  
LANSING DUNCAN  
ANN GLASGOW  
EILEEN WYCKOFF  
TOM HOM  
RANDY MELCOMBE  
HOWARD WITTAUSCH

MEMBER AT LARGE  
1<sup>ST</sup> DISTRICT  
1<sup>ST</sup> DISTRICT  
2<sup>ND</sup> DISTRICT, CHAIR  
2<sup>ND</sup> DISTRICT  
3<sup>RD</sup> DISTRICT  
3<sup>RD</sup> DISTRICT  
4<sup>TH</sup> DISTRICT  
4<sup>TH</sup> DISTRICT  
5<sup>TH</sup> DISTRICT  
5<sup>TH</sup> DISTRICT, VICE CHAIR

#### **STAFF MEMBERS PRESENT:**

Anne Rierson, Senior Deputy County Counsel  
Zoraida Abresch, Supervising Planner  
Julie Harris, County Planner III  
Rosie Dyste, County Planner III  
Anita Hodosy-McFaul, HLAC Secretary

1. **SITE VISIT:** HLAC conducted a site visit to The Coral Casino, located at 1281 Channel Drive prior to the meeting.

#### **ADMINISTRATIVE AGENDA**

**NUMBER OF INTERESTED PERSONS:** Approximately 20 people.

2. **ROLL CALL:** Everyone present.
3. **PUBLIC COMMENT:** None.

4. **MINUTES: Hearing of April 14, 2014**

**ACTION:** Commissioner Wittausch moved, seconded by Commissioner Wyckoff and carried by a vote of 11 to 0 to approve the Minutes of April 14, 2014.

5. **STAFF UPDATE:** By Zoraida Abresch, Supervising Planner and by Julie Harris, County Planner III, reporting on the status of the Hosmer Adobe:

The Hosmer Adobe Appeal was resolved at the July 1, 2014 Board of Supervisors Hearing. In the time between HLAC's action on the Hosmer Adobe and the Board of Supervisor's decision, the applicant hired an adobe preservation expert, recommended by the Pearl Chase Society, the appellant to the project. The adobe preservation expert determined the Hosmer Adobe could be rehabilitated by deconstruction and reuse of the adobe bricks, and this would still be consistent with The Secretary of Interior Standards for rehabilitation.

The Board of Supervisors denied a portion of the appeal, and also upheld a portion of the appeal by modifying the HLAC Action from approval of a demolition and reconstruction to an approval of a rehabilitation plan pursuant to the Secretary of Interior Standards.

6. **HLAC Conflict of Interest 2014 Biennial Code Review:** Ratify and approve Chair Cunningham's verification of the 2014 Local Agency Biennial Notice.

Public Comment: None.

**ACTION:** Commissioner Duncan moved, seconded by Commissioner Greenleaf and carried by a vote of 11 to 0 to ratify and approve Chair Cunningham's verification of the 2014 Local Agency Biennial Notice.

7. The Historic Landmarks Advisory Commission (HLAC) considered **County of Santa Barbara Landmark #43; The Coral Casino** proposed repairs due to significant water damage occurring as a result of faulty construction and determine whether the proposed repairs qualify as normal repair and maintenance pursuant to the landmark resolution. The proposed project includes repairs to doors, windows, roofs, eyebrows, decks, guardrail walls, glass railing and repairs to construction defects causing water intrusion. The property is shown as Assessor's Parcel Number: 009-353-015 located at 1281 Channel Drive (situated across from the Biltmore Hotel) in the Montecito area, First Supervisorial District.

Determine that the above action is exempt from CEQA under Section 15331 of the CEQA Guidelines, because the project is limited to repairs of historical resources in a manner consistent with the Secretary of the Interior's Standards for the Treatment of Historic Properties with Guidelines for Preserving, Rehabilitating, Restoring, and Reconstructing Historic Buildings, (1995), Weeks and Grimmer.

**Present:**

**Julie Harris, County Planner III**

**Steve Welton, Suzanne Elledge Planning and Permitting Services**

**Clay Aurell and Josh Blumer, Architects, AB Design Studio, Inc.**

**David Wessel, architectural historian and principal from Architectural Resources Group (ARG), project historian on the Coral Casino renovation.**

Public Comment: Kellam de Forest

**Presentation:**

**Julie Harris** distributed a brief memo to HLAC members with a copy of Conditions of Approval that HLAC applied to the Coral Casino Rehabilitation project in 2005.

**Steve Welton, Clay Aurell, Josh Blumer and David Wessel:**

The Commission received an application from the Coral Casino representatives that included Preservation General Notes and a Preservation Protocol Plan. The repairs will be done in phases, so the Coral Casino will not be shut down entirely during construction using the



existing 2005 Conditions of Approval. A Substantial Conformity Determination and Coastal Development Permit will also be required.

A graphic presentation indicating the work to be done was shown to the Commission. The representatives acknowledged that there would be no new details and work done to the Coral Casino other than the repairs consistent with the Secretary of State Interior Guidelines. Most of the repairs would be concealed, follow an established protocol plan and include documentation of deficiencies of the first installation to avoid future problems.

**ACTION:** Commissioner Duncan moved, seconded by Commissioner Lenvik and carried by a vote of 11 to 0 to approve the proposed repairs and to address the significant water damage to the Coral Casino under the guidance of the protocol plan and the existing Conditions of Approval, to include monitoring conditions consistent with historic preservation and determine that the above action is exempt from CEQA under Section 15331 of the CEQA Guidelines.

8. Presentation from Post Hazeltine Associates on preliminary results of the HLAC Survey for Fiscal Year 2013/2014:
  - Preliminary results were reported by Tim Hazeltine and Pamela Post.
  - Final Survey results will be submitted during the October 13, 2014 meeting.
9. Long Range Planning Division presented the **Mission Park to Mission Canyon Multimodal Improvements Project (Mission Canyon Corridor Project)** to the Historic Landmarks Advisory Commission for courtesy review and comments.

Rosie Dyste, Planner III from County Long Range Planning Division presented HLAC with the details of the proposed improvements along the Mission Canyon corridor. The corridor begins at Laguna Street and Los Olivos Street. Los Olivos Street becomes Mission Canyon Road at the intersection of Alameda Padre Serra and the corridor ends at the intersection of Mission Canyon Road and Foothill Road.

The City of Santa Barbara has jurisdiction over the lower portion of the corridor and the corridor becomes the jurisdiction of the County of Santa Barbara approximately around the intersection with Alameda Padre Serra. A team comprising of County and City Planners, Interest Groups and the private property owners have been involved in the process. The Mission Canyon Heritage Trail Association was present at the meeting. Agreements have been established with Mission Santa Barbara, Mission Historical Park, Santa Barbara Museum of Natural History, Mount Calvary, residents Dr. and Mrs. Kay, Rocky Nook Park and the Santa Barbara Women's Club. The project is a grant funded partnership between The County and the City of Santa Barbara. It is a long range effort and will take 10 to 20 years to complete. Public participation is crucial; two public workshops were held to present concept plans for the corridor. All potential historic and archaeological impacts would be evaluated at a future phase if the conceptual project is pursued and funded by decision-makers.

**Public Comments:**

Kellem de Forest, No position  
Fred Sweeny; In favor  
Karl Hutterer; In favor  
Tom Jacobs; In favor  
Nik Schiffmann; In Opposition  
Lanny Eberstein; In Opposition

**HLAC COMMENTS:**

- Concerns that the use of standard widths for roads will not maintain the scale of the rural area and that widening and straightening of the corridor will allow and encourage unsafe traffic speed. The plan would need study to create traffic calming measures not widening of

- the lanes to slow traffic and to improve safer pedestrian and bicycle traffic in scale without disturbing the rural nature of the corridor.
- Cluster of trees and bridge are historic and necessary obstructions and are a gateway from the City of Santa Barbara and the County of Santa Barbara. The trees and bridge are necessary for traffic calming and shifting pedestrian circulation at pertinent crossings would also enhance traffic safety.
  - Further study for safety and aesthetics is required for the pedestrian path and bridge, and the mitigation of the road.
  - The stegosaurus wall which some historians have determined to be eligible for landmark status should not be manipulated or shortened. More study is critical to protect and preserve historic walls.
  - Consensus from HLAC for further study on safety impacts for pedestrian, bicycle paths, ADA requirements, and traffic calming, while preserving the historic and rural theme of the scenic corridor.
10. **HISTORIC RESOURCES SURVEYS:** Discussion and consideration of Historic Resources Survey Proposals for Fiscal Year 2014/2015.
- An ad hoc subcommittee was formed with Commissioners Wittausch, Lenvik and Tomson.
  - HLAC tabled the discussion and consideration to the October 10, 2014 HLAC meeting.
11. **DISTRICT REPORTS:** None.
12. **NEXT MEETING of the Historic Landmarks Advisory Commission:**
- |                  |  |
|------------------|--|
| <b>Date:</b>     | October 13, 2014   |
| <b>Location:</b> | Santa Barbara County Planning Commission Hearing Room<br>Engineering Building, Room 17<br>123 East Anapamu Street<br>Santa Barbara, CA 93101 |

Respectfully submitted by,

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Anita Hodosy-McFaul, Secretary, HLAC





# Santa Barbara County Park Commission

610 Mission Canyon Road Santa Barbara, California 93105 (805) 568-2461

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## SANTA BARBARA COUNTY PARK COMMISSION

Meeting of August 28, 2014

### MINUTES

The regular hearing of the Santa Barbara County Park Commission was called to order by Chair Fran Farina at 9:34AM., at the Board of Supervisors Conference Room, 105 E. Anapamu Street, 4<sup>th</sup> Floor, Santa Barbara, CA 93101.

#### Commissioners Present:

Suzanne Perkins	1 <sup>st</sup> District, Vice Chair
Fran Farina	2 <sup>nd</sup> District, Chair
Judith Dale	3 <sup>rd</sup> District
James Mosby	4 <sup>th</sup> District

#### Commissioners Absent:

Bailey Hudson	5 <sup>th</sup> District
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#### County Staff Present:

Renée E. Bahl, Interim Director  
Paddy Langlands, Deputy Director  
Michael Allen, North County Operations Manager  
Jeff Lindgren, South County Operations Manager  
Jon Menzies, Aquatics Coordinator  
Jill Van Wie, Capital Projects Manager  
Ryder Bailey, Cost Analyst  
Kerry Bierman, CSD CFO  
Claude Garciacelay, Planner  
Rosie Dyste, Planning and Development  
Jessica Rauch, Administrative Professional

#### ADMINISTRATIVE AGENDA:

- I. **HEARING CALLED TO ORDER:** by Chair, Fran Farina.
- II. **ROLL CALL:** Commissioner Hudson was absent.



**III. PLEDGE OF ALLEGIANCE**

**IV. MINUTES:** The Minutes of May 22, 2014 were considered as follows:

**ACTION:** Commissioner Dale moved, seconded by Commissioner Mosby and carried by a vote of 3-0-1 (Perkins abstained, Hudson absent) to approve the Minutes of May 22, 2014.

**V. PUBLIC COMMENT:** None.

**VI. PARK COMMISSIONER'S NON-AGENDA ITEMS:** Commissioner Mosby had a Lompoc resident inform him of a liquid oozing from one of the concrete slabs at Jalama. He has not received pictures yet. Commissioner Mosby would also like a presentation at the next meeting on the completion of the Miguelito Restroom project.

**VII. OPERATIONS AND FISCAL STATUS REPORTS:** The Commission received and filed reports from staff.

**VIII. DIRECTOR'S REPORT AND BOARD OF SUPERVISORS HEARING SUMMARY:** Ms. Bahl announced that Chief Financial Officer, Kerry Bierman will be going to the District Attorney's Office. Mr. Langlands commented on the water conservation steps the department is taking; one of which is the outdoor showers at the beaches will be shut off. There was a homicide at Guadalupe Dunes, which shut down the park for a period of time. The Isla Vista fencing project is moving forward and will be going to the Planning Commission in October. Staff has been working with outside agencies on a Memorandum of Understanding to restore the native plants. The Cuyama pool will not be open for next summer because of insurance issues.

**IX. STANDARD AGENDA:**

**1. Mission Canyon Corridor Improvements Project – For Information**

Recommendation: That the Park Commission receive a presentation on the Mission Canyon Corridor Project from Planning and Development Project Manager, Rosie Dyste.

**ACTION:** The Park Commission received a presentation on the Mission Canyon Corridor Project from Planning and Development staff.

**2. Board Approved Preventative Maintenance – For Information**

That the Park Commission receive and file a presentation on the Board of Supervisors approved preventative maintenance.

**ACTION:** The Park Commission received a presentation on the Board approved preventative maintenance for County parks.

**X. FUTURE MEETING AGENDA ITEMS**

Commissioner Mosby would like presentations on the Miguelito Project, update on the Bodger Trail and a presentation on the Ocean Park development trail.

**XI. ADJOURN**

**MOTION TO ADJOURN:** Commissioner Perkins moved, seconded by Commissioner Dale and carried by a vote of 4-0 (Hudson absent) to adjourn the August 28, 2014 Park Commission meeting.

**Adjourned at 10:36AM**

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**Renee E. Bahl**  
**Interim Director of Community Services**



ATTACHMENT G

**City of Santa Barbara**  
**Planning Division**

**HISTORIC LANDMARKS COMMISSION**  
**MINUTES**

**Wednesday, August 27, 2014**      **David Gebhard Public Meeting Room: 630 Garden Street**      **1:30 P.M.**

**COMMISSION MEMBERS:**

PHILIP SUDING, *Chair*  
BARRY WINICK, *Vice-Chair*  
MICHAEL DRURY  
WILLIAM LA VOIE  
BILL MAHAN  
FERMINA MURRAY  
JUDY ORÍAS  
CRAIG SHALLANBERGER  
DONALD SHARPE

**ADVISORY MEMBER:**

DR. MICHAEL GLASSOW

**CITY COUNCIL LIAISON:**

DALE FRANCISCO

**PLANNING COMMISSION LIAISON:** SHEILA LODGE

**STAFF:**

JAIME LIMÓN, Design Review Supervisor / Historic Preservation Supervisor  
NICOLE HERNÁNDEZ, Urban Historian  
MICHELLE BEDARD, Assistant Planner  
GABRIELA FELICIANO, Commission Secretary

**Website:** [www.SantaBarbaraCa.gov](http://www.SantaBarbaraCa.gov)

An archived video copy of this regular meeting of the Historic Landmarks Commission is viewable on computers with high speed internet access on the City website at [www.SantaBarbaraCA.gov/HLCVideos](http://www.SantaBarbaraCA.gov/HLCVideos) then clicking on the Videos under Explore.

**CALL TO ORDER.**

The Full Commission meeting was called to order at 1:30 p.m. by Chair Suding.

**ATTENDANCE:**

Members present: Drury, La Voie, Mahan, Murray, Orías, Sharpe, Suding, and Winick.  
Member absent: Shallanberger.  
Staff present: Limón (until 3:32 p.m. and again at 6:00 p.m.), Hernández (until 6:40 p.m.), and Feliciano.

**GENERAL BUSINESS:**

**A. Public Comment:**

No public comment.



Staff agrees with the report that the project meets the Secretary of Interior Standards for Rehabilitation and will not have a negative impact on the resource meeting the CEQA guidelines. Because the project is concentrated in the rear portions of the property and that it replaces non-historically significant features and that it will be set back behind the historic house so that it will not detract from the historic resource. The proposal is compatible in features, size scale, proportion and massing to the historic resource.

(Additional staff notes: The house is eligible to be designated a City Landmark for criteria a,d,e,f,g,h and I and 5 and 6; eligible for listing on the California Register of Historic Places and National Register of Historic Places.)

**Motion:** To accept the report with an amendment to the last sentence on page 21.

**Action:** La Voie/Mahan, 8/0/0. (Shallanberger absent.) Motion carried.

**b. Project Concept Review (Comments only; project requires environmental assessment and Staff Hearing Officer review for requested Zoning modifications.)**

**Motion:** Continued indefinitely to the Staff Hearing Officer with comments:

1. The proposed modification is aesthetically appropriate. The proposed modification does not pose consistency issues with El Pueblo Viejo Design Guidelines or the Neighborhood Preservation Ordinance.
2. The proposed changes to the residence are supportable.
3. The property owner is encouraged to consider landmark designation.

**Action:** La Voie/Orías, 8/0/0. (Shallanberger absent.) Motion carried.

**DISCUSSION ITEM**

**6. 400 BLK E LOS OLIVOS ST 473 SEG ID**

(5:20) Assessor's Parcel Number: ROW-000-473  
Application Number: MST2014-00406  
Owner: City of Santa Barbara  
Applicant: Rosie Dyste  
Engineer: Public Works Department

(Proposed improvements to the Mission Canyon Corridor, including ADA accessible path, pedestrian bridge, and roadway and wall alignment adjustments.)

**(Discussion only on the Mission Canyon Corridor Community Consensus process.)**

Actual time: 5:22 p.m.

**Present:** Rosie Dyste, Senior Planner, County of Santa Barbara Planning & Development  
Rob Dayton, Principal Transportation Planner, Public Works Department

**Staff comments:** Nicole Hernández, Urban Historian, stated that the purpose of the discussion is to determine if the proposed location of the new pathway is appropriate; and a historic structures report will be prepared once the location is determined.

Public comment opened at 5:35 p.m.

1. Larry Ebenstein, local resident, spoke in opposition to the project and commented that it would destroy the current feeling and streetscape of Mission Canyon.
2. Fred Sweeney, local resident, Mission Heritage Trails Association, expressed support of the project and commented on the association's members, purpose and process.
3. Heidi Jones, Mission Heritage Trails Association, commented that the group's concerns about historic preservation has been a key part of the process from the beginning and reaffirmed Shelley Bookspan's written comments.
4. Kellam de Forest, Mission Heritage Trails Association, commented that the purpose of the association was originally due to concerns with the safety for children, senior citizens and ADA accessibility for pedestrians walking from the Mission to the Museum of Natural History, while preserving the existing historic resources.

Chair Suding acknowledged receipt of written comments from Alexandra Rosenthal, Paulina Conn and Shelley Bookspan and summarized each.

Public comment closed at 5:48 p.m.

**Discussion held with the comments:**

1. A historic structures report is necessary to make a final determination as to the proposed re-alignment or relocation of the two historic walls. The Commission considers the two walls extremely important and they are sensitive historic resources.
2. The Commission supports the corridor improvement project as doable and consenting to the idea of a solution too, but the project must maintain the rural and rustic character of the existing roadway. Meandering paths separate from the roadway should be considered rather than traditional sidewalks.
3. The existing masonry bridge is also a sensitive historic resource. However; the Commission does not object to the idea of a separate footbridge on the downstream side of the masonry wall.
4. The Commission requests the applicants continue to work with the community on design solutions that consider the HLC's expressed concerns.
5. Some Commissioners expressed strong objections regarding moving the walls and recommended that a goal be the preservation of resources. Yet, further analysis of the project would be necessary if there are possible impacts to these historic resources.





## MEETING MINUTES

CITY OF SANTA BARBARA

### TRANSPORTATION AND CIRCULATION COMMITTEE (TCC)

David Gebhard Public Meeting Room  
630 Garden Street, Santa Barbara, CA  
Thursday, October 23, 2014, 6:00 PM

CALL TO ORDER: Vice Chair Boche called the meeting to order at 6:00 PM

#### ROLL CALL:

##### TCC MEMBERS

Hillary Blackerby  
Cynthia Boche  
Bob Burnham  
Keith Coffman-Grey  
Edward France  
Susan Horne  
Kathleen Rodriguez

##### Attendance

Present  
Present  
Present  
Present  
Present  
Absent  
Present

##### CITY STAFF PRESENT :

Browning Allen, Transportation Manager  
Rob Dayton, Principal Transportation Planner  
Kim Thaler-Strange, Administrative Specialist  
Peter Brown, Acting Mobility Coordinator  
John Ewasiuk, Principal Civil Engineer  
Jessica Grant, Project Planner  
Derrick Bailey, Supervising Trans. Engineer

##### LIAISONS PRESENT

##### PUBLIC PRESENT:

Don Olson, Mission Heritage Trail Association  
Matt Dobberteen, Santa Barbara County  
Steve Maas, Metropolitan Transit District

#### CHANGES TO THE AGENDA:

##### 1. PUBLIC COMMENT:

Ed France, Director of the Bicycle Coalition announced that daylight savings time is next week, and that the Bicycle Coalition will be giving out bike lights. It is required by law to have a light in front. Illuminada de la Noche will be at 5 locations.

Derrick Bailey, Supervising Traffic Engineer announced that on Wednesday, November 5 at 6:00 p.m., there will be a public workshop at Monroe School on the Mesa to discuss the Cliff Drive Traffic Improvement Project. This item will be coming to the TCC in 2015. There will also be a virtual public meeting.



## TRANSPORTATION AND CIRCULATION COMMITTEE

Meeting Minutes

October 23, 2014

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circulation element. People living in the corridor don't drive often. There is the 75 min shuttle and street sweep shuffle, which is a very common complaint. People got tickets for street sweeping. Car share can fill that need. Great time it is for this topic because local jurisdictions UCSB, IV, Airport have established carshare programs. Very timely and consistent with circulation element.

HB: good thing, and very smart and could work in community. Think it is consistent with circ. Element. Long past time for this.

CFG: Consistent with circ element. Glad to see city moving forward with this option.

Motion: move that we find this motion 6 ayes no opposed and 1 absent (horne)

### 6. Mission Canyon Corridor Community Consensus Proposal

Rob Dayton, Principal Transportation Planner gave this presentation to the Committee. He gave an overview of the project, and the community outreach that has been done to find the issues that the community would like fixed. He then described the process, and the grant that was received by Santa Barbara County. The City, County and Caltrans will be working together to move the project forward for consensus. There have been listening workshops to discuss the items important to the area such as: history of the area, resources, connectivity, what isn't working, what needs to be fixed, what items need to be left alone. Controversial items were not considered.

He introduced Don Olson of the Mission Heritage Trail Association, and Matt Dobberteen of the County who was present to answer questions, and showed drawings and plans done by Fred Sweeney who is the architect for this project. There will be a Joint City Planning Commission/County Planning Commission meeting on this item on December 10, 2014, at the County.

#### COMMENTS FROM TCC

Kathleen Rodriguez was confused and wanted clarification as to the disposition of a footbridge that is opposite the bridge. Mr. Dayton replied that leaving the bridge is an option, but it wouldn't impact the goal. Ms. Rodriguez asked if there was a possibility of asking for easements from residents. According to Mr. Dayton, while that is possible, there are some walls that would not be touched. He added that there is room between the road and the walls, but the challenge is the trees and undulation.

Hillary Blackerby is excited to see the presentation on this; there are so many conflicts in the area. It's cool to see the County and City working with the community on this. She asked about the footpath, and what surface it would be. Mr. Dayton replied that they have avoided discussions of design details for now; that would be a resolution between Council and the Board of Supervisors.

Ed France asked if the side being shown is the switch from the City to the County at Mission Creek? Mr. Dayton indicated that the County's section of roadway begins at the end of the bridge. All the properties on the outside of the roadway, except the Kay property are part of the City. What the Planning Commission would be doing as part of the Natural History Museum

project is an annexation of this road all the way up to Las Encinas so that we will be responsible for access.

Ms. Rodriguez commented that this should be a high priority for safety and tourism.

Ms. Boche was at the initial listening workshop and was impressed that staff was able to find something to do with the information given, and come up with this project. It is a great start on improving this corridor, and is Consistent with the Circulation Element. She hopes it will be a high priority project.

Mr. France appreciated the process, and seeing the Transportation Planning Division at a community level and seeing both the County and the City doing a superb job of accommodating and integrating into the public process. He is also positively surprised by a functional improvement for bicycling and walking. This is how it is supposed to happen.

Mr. Coffman-Gray pointed out that Santa Barbara has so many historical areas. To get the County, City and the neighbors behind a plan has seemed insurmountable, but is glad to see everyone working together. He agrees that it is within the Circulation Element for improving access to the area. There are so many tourists, and this is one of the top 3 areas they go to. It is important to keep the historical nature of the area.

Mr. Burnham is in agreement that this project is consistent with the Circulation Element. He asked if once this is started, will it be continued as far as community involvement? Mr. Dayton replied that the next step is a Joint Planning Commission meeting. The session will be a rich public process. Staff will also go back to the Historic Landmarks Commission, followed by a more public process at Council and the Board of Supervisors. If they are told to find funding, there will be another rich community process to design a project. He also said that this will be at Council probably at the beginning of next year. This has to be completed by February for the grant. If it moves forward, the City will be applying for grants, and finding projects for grant opportunities.

**Motion:** That the Transportation and Circulation Committee recommend to City Council that the Mission Canyon Corridor Community Census Proposal is consistent with the Circulation Element of the General Plan

Motion made by Blackerby and seconded by Rodriguez.

Ayes; 6      Noes:      Absent (1) Horne      Abstain:

## **7. Status Report on the Bicycle Master Plan Engagement Process**

Peter Brown, Interim Mobility Coordinator gave the following update: Staff just completed the RFP phase and will be reviewing proposals next week. Mr. Allen and Mr. Dayton will be selecting three to four firms to interview and will select one, and take an Award of Contract to Council. We will hit the ground running in 2015.

Mr. France asked when the RFP recommendation will go to Council. Mr. Allen said we just received the proposal the previous day and still need to interview. It is possible that staff could go to Council in early December, but a selection needs to be made, and a contract negotiated.



## ATTACHMENT I

### RESOLUTION OF THE BOARD OF SUPERVISORS COUNTY OF SANTA BARBARA, STATE OF CALIFORNIA

IN THE MATTER OF DIRECTING COUNTY )  
STAFF TO WORK COLLABORATIVELY WITH )  
CITY OF SANTA BARBARA STAFF TO PURSUE )  
FUNDING OPPORTUNITIES TO IMPLEMENT ) RESOLUTION 15-\_\_\_\_  
THE MISSION PARK TO MISSION CANYON )  
MULTIMODAL IMPROVEMENTS PLAN )

#### WITH REFERENCE TO THE FOLLOWING:

1. For decades, community concerns have been expressed regarding safety for all modes of travel through from the intersection of Laguna Street and East Los Olivos Street at Mission Santa Barbara to the intersection of Mission Canyon Road and Foothill Road (Mission Canyon corridor), but little has been done because of the sensitive nature of the historic and environmental resources of the corridor.
2. In 2011, community members formed the “Safe Passage Working Group,” a volunteer citizens organization of diverse interests now known as Mission Heritage Trail Association, and worked to build community consensus regarding issues of concern along the Mission Canyon Corridor between Laguna Street and Foothill Road.
3. On August 15, 2012, Caltrans awarded the County of Santa Barbara, hereinafter referred to as “County,” and the City of Santa Barbara, hereinafter referred to as “City,” a Community-Based Transportation Planning Grant for the Mission Park to Mission Canyon Multimodal Improvements Plan, hereinafter referred to as the “plan.”
4. The County and City used the Caltrans grant funds to work in partnership on public outreach to determine and document community concerns and preferences and prepare concept plans for circulation improvements to Mission Canyon corridor. Through two well-attended workshops, staff was able to identify minimum-level improvements with overwhelming community acceptance, while addressing the community-identified safety concerns.
5. The plan is a community consensus concept level design for multimodal improvements consistent with County and City policy direction for the project area (County Mission Canyon Community Plan, City Circulation Element and Pedestrian Master Plan).
6. Based on public input, the County and City prepared the concept level designs for continuous pedestrian access on the west side of the corridor and improved circulation for bicyclist passage and vehicle safety. The plan includes the following key components:



- Continuous pedestrian path on the west side of the corridor. The path must comply with the ADA and be consistent with the historical setting.
  - Landscape buffers between path and roadway, where feasible.
  - New detached pedestrian bridge parallel and west of the existing stone bridge over Mission Creek.
  - Bike lanes extending north to Puerta del Sol transitioning into bike routes in the County's jurisdiction where the road narrows.
  - Relocation of several crosswalks.
  - Left-turn pocket from Mission Canyon Road to Las Encinas Lane.
7. From July 2014 to October 2014, the concept plans were reviewed by five County and City Boards and Commissions (County South Board of Architectural Review, County Historic Landmarks Advisory Commission, County Park Commission, City Historic Landmarks Commission, and City Circulation and Transportation Committee).
  8. On December 10, 2014, the County and City Planning Commissions jointly held a duly noticed public hearing on the plan, at which hearing the concept plans were explained and comments invited from the persons in attendance.
  9. The Board of Supervisors received and considered the County Planning Commission's recommendations at a duly noticed public hearing on the plan where the concept plans were explained and comments invited from the persons in attendance.
  10. It is in the beneficial interest of all parties to collaborate in future phases of improvements to the Mission Canyon corridor in order to share staff expertise and existing information, to promote intergovernmental coordination, and to serve the public interest by producing a more efficient project in both jurisdictions.

**NOW, THEREFORE, IT IS HEREBY RESOLVED** as follows:

1. The above recitations are true and correct.
2. The Board of Supervisors directs County staff to work collaboratively with City of Santa Barbara staff to pursue funding opportunities to implement the Mission Park to Mission Canyon Multimodal Improvements Plan, including but not limited to:
  - Formal survey of the road right-of-way, where required.
  - Preliminary design engineering.
  - Environmental review pursuant to CEQA.
  - Review by County and City committees, where needed.
  - Permits and/or rights-of-way, where needed.
  - Final design engineering.

PASSED, APPROVED, AND ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2015 by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

\_\_\_\_\_  
CHAIR  
BOARD OF SUPERVISORS

ATTEST:

MONA MIYASATO, COUNTY EXECUTIVE OFFICER  
CLERK OF THE BOARD

By \_\_\_\_\_  
Deputy Clerk

APPROVED AS TO FORM:

MICHAEL C. GHIZZONI  
COUNTY COUNSEL

By \_\_\_\_\_  
Deputy County Counsel



# Mission Park to Mission Canyon Multimodal Improvement Project

## Mission to Museum Conceptual Design Study

Public Works Department,  
Engineering Division,  
City of Santa Barbara

  
R. Patrick Kelly  
Assistant Public Works  
Director/City Engineer

  
Andrew Grubb  
Project Engineer



December 2014



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## **Background**

In September 2012, a community group (Concerned Citizens for Safe Passage - Safe Passage) made a presentation to Council requesting that the City of Santa Barbara (City) support the improvement of pedestrian circulation in the Lower Mission Canyon area. They asked the City and the County of Santa Barbara (County) to work together to study ways to improve the safety of pedestrians passing between the intersection of Los Olivos and Laguna Streets near the Santa Barbara Mission (Mission) in the City, and the intersection of Mission Canyon Road and Foothill Road in the County's jurisdiction. This corridor passes several historic and otherwise notable landmarks, including the Mission, Mission Historical Park, Rocky Nook Park, the Santa Barbara Museum of Natural History (Museum), and the Santa Barbara Woman's Club (Women's Club).

Council directed staff to work with the community group and the County to create a plan that will work towards developing a community consensus for solutions to improve pedestrian circulation in that area. This collaborative planning and conceptual design process was funded through a recent County acquired Caltrans planning grant for a Mission Canyon Corridor Multimodal Improvement Project (Project) with the City as a 'sub-applicant'. Rob Dayton, Principal Transportation Planner, was assigned as the lead City staff member in this planning process, acting as a resource at all meetings and coordinating and giving presentations to City Boards and Commissions.

Subsequently, it was recognized that professional engineering services would be necessary to help with conceptual design of the project's multimodal elements and with construction cost estimates. In March 2014, the City contracted with the County to provide professional services for conceptual design of multimodal improvements for the Project. The City's scope of work would provide technical report in support of the Mission Park to Mission Canyon Multimodal Improvements Report that the County is preparing as part of the grant. The County and City agreed that City staff would focus on a study area for the portion of the Los Olivos Street and Mission Canyon Road corridor between the Mission and the Museum. This stretch of the corridor between the Mission and the Museum is primarily within the City's jurisdiction (See Exhibit A – Mission to Museum Photo Map).

## **Purpose**

The purpose of this City report was focused on studying improved pedestrian access between the Mission and the Museum, while still accommodating appropriate vehicular and bicycle roadway geometries per city, state, and federal design standards. In addition, the City studied feasible alternatives to improve pedestrian access across Mission Creek, as well as improved pedestrian crossing along the Los Olivos Street and Mission Canyon Road corridor.

As part of the planning process, several public outreach meetings were held to gather input on pedestrian, bicyclist, and vehicular improvements along the corridor (See Public Meeting Summaries, Exhibits F1 –F3 and G1-G4). City staff coordinated with the County and Safe Passage to develop and supply exhibits for these public meetings.

## **Design Constraints**

Several design constraints were identified during this planning study for creating improved pedestrian, bicycle, and vehicular use along the corridor study area between the Mission and the Museum (See Exhibit A – Mission to Museum Photo Map). These two sites attract a significant number of visitors who attempt to walk the approximately quarter to half mile between them. Many pedestrians have been observed to turn back once they see there is not a conventional and continuous sidewalk between them. The City identified many key impediments to consider for designing a continuous pedestrian path as outlined below.

## **Historic Features**

The Mission Creek Bridge (Bridge) was constructed in 1891 by the adjacent property owner and the County, having a 22 foot wide roadway deck and masonry railings. A wood plank pedestrian footbridge was added by the County on the east side in 1929, modifying the railing to accommodate pedestrians using the footbridge. The bridge was widened on the westside in 1930 to its current 30 foot roadway deck dimensions, which required shifting the walls immediately abutting the bridge on the west, several feet to accommodate the widening.

In the 1980s, the bridge was determined by Caltrans to be eligible for listing on the National Registry of Historic Places. A Historic Resources Study is in progress to attempt to verify the nature and context of any improvements at the Bridge and immediately adjacent to it. Any improvements that are made adjacent to the Bridge will have to take in to consideration the effect on the historical significance of the structure.

Other key historic features of the study area include the Oliver Memorial Trough abutting the eastside footbridge, and the the Mission Aqueduct (Aqueduct). The Aqueduct was built in the early 1800's and was once connected to the historic Lower Reservoir to the east. These historic features essentially frame the southerly edge of the Los Olivos Road and Mountain Drive intersection and limit the space to 38 feet for the paved vehicular roadway, a pedestrian path, and bicycle lanes. Due to the narrow space here, this becomes the key 'pinch point' for allowing pedestrian access along the corridor. The Aqueduct is believed to be of such historic significance that it cannot be touched.



## Pedestrian Accessibility

Pedestrian access through the corridor is currently limited and unimproved. A continuous and accessible sidewalk for pedestrians does not exist in the corridor. The current pedestrian route varies between roadway shoulders, hardened dirt paths, park paths, degraded asphalt, stairs, a wooden footbridge, and vehicular roadway that pedestrians are forced to navigate in order to get from the Mission to the Museum. The following outlines the current pedestrian route on the West and East sides of Los Olivos Street and Mission Canyon Road:

### Westside Path Option:

1. Starting at the paved plaza in front of the Mission, there are steps, but no access ramp to the Los Olivos Street westerly parkway path to the Bridge.
2. A degraded asphalt path leads from Laguna Street north on the west side of Los Olivos Street and ends abruptly at the driveway that currently serves St. Mary's Retreat House. The path surface does not meet modern accessibility standards.
3. The three driveways along the westerly path to the Bridge exceed the maximum cross slope for modern accessibility standards.
4. North of the driveways, the westerly path continues as a compacted dirt surface until it terminates at the Bridge.
5. There is a 'pinch point' to about 3.5 feet on the westerly path between the roadway curb and a remnant of the historical Aqueduct just south of the Bridge at the path's termination.
6. At the Bridge, a pedestrian is forced to cross the approximate 300-foot long and 30-foot wide bridge. Privately-owned stone walls block access to Mission Creek, which is a deeply incised channel at this location.
7. If continuing on the vehicular roadway, a pedestrian is forced to walk into oncoming traffic with limited visibility, due to a curve in the road, to go to the other side or cross the 30-foot roadway to the easterly side, where a wooden catwalk attached to the Bridge steps down to cross over Mission Creek to Rocky Nook Park and added dirt pathways. (see Eastside Path option below)
8. To the north of the Bridge, a narrow dirt roadway shoulder varies from a few inches to up to about four feet wide between the Privately-owned "Stegosaurus" stone wall and the edge of the paved road.
9. The narrow shoulder contains a driveway entrance to the adjacent property and two power poles, before approaching the Puesta del Sol and Mission Canyon Road intersection.
10. The southerly side of Puesta del Sol currently does not contain an improved pathway. In order to access the Museum entrance, pedestrians are forced to continue on the asphalt shoulder that currently serves as a parallel parking lane, avoiding rocks, trees, a driveway, and other obstacles.

#### Eastside Path Option:

1. Starting at the stop sign-controlled Los Olivos/Laguna Street intersection in front of the Mission, there is a crosswalk that leads to the Mission Park on the east side of Los Olivos Street.
2. A narrow degraded asphalt path continues on the east side of Los Olivos Street, adjacent to the Mission Historical Park, until it ends abruptly prior to an existing stacked stone wall.
3. A pedestrian is then forced into the asphalt shoulder for approximately 230 feet to an uncontrolled crosswalk to the Mission.
4. In order to continue north on the sidewalk on the east side of Los Olivos Street, a pedestrian is forced to navigate the three-way Y-intersection with Alameda Padre Serra and an uncontrolled through-right turn travel lane that doesn't contain crosswalks.
5. Continuing northerly toward the Museum and Rock Nook Park, there are several uneven paths on the edge of the road and through the Mission Historic Park, where the path ends abruptly at the stop sign for Mountain Drive. After crossing Mountain Drive, there is a wooden catwalk attached to the eastside of the Bridge to cross to the entrance of Rocky Nook Park.
6. The eastside path continues as a compacted dirt surface adjacent to Rocky Nook Park with various pathway obstacles on the east side of Mission Canyon Road to the intersection with Puesta del Sol.
7. At the Puesta del Sol and Mission Canyon Road intersection, pedestrians must continue across the Woman's Club driveway entrance to an uncontrolled crosswalk in order to cross to the west side of Mission Canyon Road.
8. Once on the west side of the street, pedestrians must cross Puesta del Sol at the stop sign controlled T-intersection to get to the south side of Puesta del Sol to access the Museum.

#### Mission Creek Bridge:

The Bridge generally marks the end of Los Olivos Road in the City and the beginning of Mission Canyon Road in the County. Since the bridge roadway deck is only 30 feet wide, which is just enough room for two traffic lanes and two bike lanes, there is not enough room to provide exclusive space for pedestrians. For pedestrians crossing on the eastside of the Bridge, there is an existing wooden footbridge to cross the creek. The footbridge approaches do not meet current accessibility standards. Pedestrians crossing on the westside of the bridge must use the existing roadway shoulder to cross the creek.

Per the Caltrans Bridge Inspection Report for the Bridge, its deck geometry (essentially the width for the amount of traffic) and approach roadway alignment is assessed as being "functionally obsolete" by Caltrans. Planning and design studies are programmed in the 2014-2015 Federal Transportation Improvement

Program (FTIP). The City is scheduling to request authorization for Caltrans to begin the design work in Winter/Spring 2015.

### **Conceptual Design Study**

A public workshop was held on October 29, 2013, at the Woman's Club to gather community input about the corridor (See Exhibit F-1 – 3, First Public Workshop Summary).

Due to the minimum space along the study area and the tight physical constraints and tolerances to meeting minimum vehicular, bicycle, and pedestrian passage along with the curving roadway, it was important to have an accurate survey. Therefore, the City arranged to compile City and County roadway surveys. The City did add some spot surveys at its own cost for added detail.

After considering all the input from the first public workshop and physical constraints, a conceptual design proposal was developed. The complete AutoCAD conceptual design study can be found in Exhibit B – Concept Plans for reference. This civil design work was used as the basis for the architectural rendering that was presented to the public at the second public workshop in April 2014 where it received overwhelming support (89%) from community participants. (See Exhibit G-1 – 4 Second Public Workshop Summary).

The following summarizes the conceptual design study performed for the community consensus proposal:

#### **Continuous Pedestrian Pathway**

As stated earlier, the primary focus of the planning study was to develop a continuous pedestrian pathway from the Mission to the Museum. City staff performed several design studies to develop a community consensus proposal. The proposed pathway stays on the westside beginning at the Laguna Street/Los Olivos Street intersection.

Currently on the westside, where there is a pathway, it is degraded asphalt or hard-pack dirt. To meet current accessible standards, several new sidewalk surfaces were considered in this study (See Exhibit C-5 Sidewalk Surface Studies). Sidewalk edge types were also considered and studied for use (See Exhibit C-4 Sidewalk Edge Studies). It is anticipated that further design studies will present these to the various boards and commissions for final design.

#### **Mission Creek Bridge - Pedestrian Bridge Alternatives**

In order to provide a continuous pathway on the west side, a portion of the two adjoining parcels would have to be acquired in order to modify the existing



privately-owned stone walls and create room for a pathway and separated pedestrian bridge. The City and County have conducted positive discussions with the current property owners and the south wall is part of the St. Mary's Retreat House (505 East Lost Olivos Street) where its local representative, Brother Tom Schultz, has expressed his support and his anticipated support from his religious monastic order. City and County staff have also met with the property owner to the north (Dr. John Kay at 609 Mission Canyon Road), who also expressed support for the project as proposed.

The costs of a new pedestrian bridge would be significantly affected by the selection of many possible prefabricated bridge options by providing a uniquely constructed parallel pedestrian bridge. Several pedestrian bridge alternatives were studied and can be seen in Exhibits C-1.1 – 1.2 and C-2.

### Roadway Realignment Study

There are several 'pinch points' in the study area for pedestrians to navigate. These are mostly described above in the Pedestrian Accessibility section of this report. The key 'pinch point', with a width of only 38 feet, is at the Aqueduct just south of the Los Olivos Street and Mountain Road intersection. Due to the 'pinch point', it is necessary to realign the roadway near the Aqueduct to create enough room for an exclusive pedestrian pathway. Due to the minimal space all along the study area and the tight tolerances to meet minimum vehicular, bicycle, and pedestrian passage, having a good survey proved to be important.

The City Traffic Engineer developed a proposed roadway alignment that includes two ten-foot vehicle lanes and two five-foot bicycle lanes (for a total roadway width of thirty feet), and enough room for a six-foot-wide accessible pedestrian pathway on the west side. It was found that roadway alignment cannot be moved sufficiently toward the west at the Aqueduct to accommodate a new pathway on the eastside while adhering to reasonable roadway curve radii.

By shifting the road slightly to the east, sufficient room can be created on the west side at the Aqueduct for a new six-foot-wide pedestrian pathway. This realignment also provides additional room at the north and south approaches to the Bridge at the private stone walls. This makes the west side the recommended alignment for a new pathway. This proposed west side pathway alignment will require moving existing private stone walls that currently connect to the Bridge rails, and building a new pedestrian bridge on the west side of the Bridge.

In addition to shifting the roadway alignment to the east at the Aqueduct, the roadway is also proposed to be shifted to the east between the Mission Creek Bridge and Puesta Del Sol. Currently, there is less than five feet between the edge of pavement and private stone wall, which is insufficient to accommodate an exclusive pedestrian pathway.

Realignment of the roadway to the east between the Bridge and Puesta Del Sol will require the relocation of several utility poles with electrical and telephone lines along the study area corridor. In addition, there are several sycamore trees that will need to be removed to accommodate the new alignment.

### Public Right of Way

As part of moving the roadway alignment to the east between the Mission Creek Bridge and Puesta Del Sol, the current east side pathway must be relocated from the hard dirt in front of Rocky Nook Park to an alignment inside the park. This is necessary to provide sufficient space along Mission Canyon Road between the Bridge and Puesta Del Sol to accommodate a pedestrian pathway and buffer (12 feet) and roadway (30 feet). This alignment will move the edge of the new roadway a few feet into the County's Rocky Nook Park property. In conversations with County staff, they feel confident that moving a new path further to the east into the Park will be compatible with the Park such that pedestrians would be walking through woodland there.

In order to accommodate the new alignment and the preferred relocation of the Puesta del Sol crosswalk, the Woman's Club driveway entrance must be modified. The iconic stone plinth columns that are located there will not be affected; however, part of the stacked stone wall in front of the plinth columns must be relocated to accommodate a new pedestrian landing for the relocated Puesta Del Sol crosswalk. The study of this re-alignment can be found in Exhibit I 2.1 – 2.4.

For the purposes of this study, the City assumed that the Museum, along with their recently proposed improvements, would reconfigure Puesta del Sol and construct the frontage improvements from the Museum entrance to the east side of the Museum's frontage. Two alternative proposals were sketched to study roadway geometrics and can be found in Exhibit C 6.1 – 6.2.

### City/County Technical Coordination

The City has been working with the County's Transportation Engineer to coordinate on the City's study area designs at the City/County limits, including new roadway alignments up to the Mission Canyon Road and Las Encinas intersection. There has been specific correspondence regarding changing the pedestrian crossing features at the Puesta del Sol intersection at the Woman's Club driveway (See Exhibit I-2.1 – 2.4). In addition, per the request of the County, the City investigated options to provide a left turn pocket on Mission Canyon Road onto Las Encinas Road (See Exhibit I 1.1 – 1.3).

## **Design Conclusions for the Community Consensus Proposal**

The City coordinated with the County to develop the proposal that was presented at the second public workshop on April 22, 2014, at the Woman's Club. The resulting Mission to Museum westside pedestrian route proposal was considered as the preferred design alternative. An eastside path that could meet accessibility standards was not considered feasible. Engineered AutoCAD based drawings were provided to a Safe Passage volunteer to create Architectural (watercolor) Renderings of the proposed design (See Exhibit E for plans and sections). Photo perspectives were also developed to conceptually show the proposed improvements (See Exhibit D).

The key components of the westside pedestrian route proposal are as follows:

- A continuous pedestrian path on the west side of the corridor (surface material to be determined). The path must comply with the Americans with Disabilities Act and be consistent with the historical setting.
- Landscape buffers between path and roadway, where feasible.
- A new detached pedestrian bridge that is parallel and west of the existing stone bridge over Mission Creek (material and structure design to be determined).
- Bike lanes extending north to Puesta del Sol that transition into bike routes in the County's jurisdiction where the road narrows.
- Relocation of the Puesta del Sol/Mission Canyon Road crosswalk.

To accomplish these improvements, the two stone walls near the bridge need to be relocated, portions of the roadway would be realigned to the east between the bridge and Las Encinas Lane, restripe the roadway in some areas, and reconstruct the roadway to accommodate grade changes necessary due to the realignment.

This proposal is considered as the minimum improvement required to provide continuous access from the Mission to the Museum. Future studies may address additional concerns that were heard at the public workshops, such as intersection improvements at Los Olivos Road and Alameda Padre Serra and the intersection of Los Olivos Road and Laguna Street, and the left-turn pocket from Mission Canyon Road onto Las Encinas Lane.

### **Initial Cost Estimates**

A conceptual cost estimate was prepared for the Mission to Museum study area with a grand total cost of approximately \$2.74 million. This figure includes a 35% construction cost contingency to accommodate the time necessary to secure construction funding and uncertainties in the final scope of construction work, as well as 30% for administration, environmental clearance, special studies, design services, and construction management (See Exhibit H-1).

## **Design Recommendations**

City and County staff's should continue to work together to further develop design and construction funding options for improving multi-modal access along the corridor. It may be necessary to make improvements incrementally as funding, grant or otherwise, becomes available to the City or the County individually or jointly.

Initial corridor multimodal improvements should include:

- A continuous pedestrian path on the west side of the corridor (surface material to be determined). The path must comply with the Americans with Disabilities Act and be consistent with the historical setting.
- Landscape buffers between path and roadway, where feasible.
- New detached pedestrian bridge parallel and west of the existing stone bridge over Mission Creek (material and structure design to be determined).
- Bike lanes extending north to Puesta del Sol that transition into bike routes in the County's jurisdiction where the road narrows.
- Relocation of Mission Canyon Road crosswalk at Puesta del Sol to the south.



## EXHIBITS - TABLE OF CONTENTS

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<b>B) CONCEPT PLANS</b>	
1) Laguna to Mission .....	B-1
2) Mission to APS.....	B-2
3) APS to Museum .....	B-3
4) Sections .....	B-4
<b>C) CONCEPTUAL DESIGN STUDIES</b>	
1) Pedestrian Bridge Site Studies .....	C-1.1 – 1.2
2) Pedestrian Bridge Structural Details .....	C-2
3) Santa Barbara Mission Access Ramp Studies.....	C-3.1 – 3.2
4) Sidewalk Edge Studies .....	C-4
5) Sidewalk Surface Studies.....	C-5
6) Santa Barbara Museum of Natural History - Puesta del Sol Pedestrian Options.....	C-6.1 – 6.2
<b>D) PHOTO PERSPECTIVES .....</b>	<b>D-1 – 3</b>
<b>E) ARCHITECTURAL RENDERINGS</b>	
1) Plans .....	E-1.1 – 1.3
2) Sections .....	E-2.1 – 2.5
<b>F) FIRST PUBLIC MEETING SUMMARY .....</b>	<b>F-1 – 3</b>
<b>G) SECOND PUBLIC MEETING SUMMARY .....</b>	<b>G-1 – 4</b>
<b>H) COST ESTIMATE .....</b>	<b>H-1</b>
<b>I) CORRESPONDENCE</b>	
1) Left Turn at Las Encinas – Letter to Rosie Dyste (June 5, 2014) .....	I-1.1 – 1.3
2) Mission Canyon Road at Puesta del Sol – Letter to Chris Sneddon (July 31, 2014).....	I-2.1 – 2.4

# A-1: PHOTO MAP – MISSION TO MUSEUM CONCEPT PLANS





## Mission Canyon Corridor Planning Process

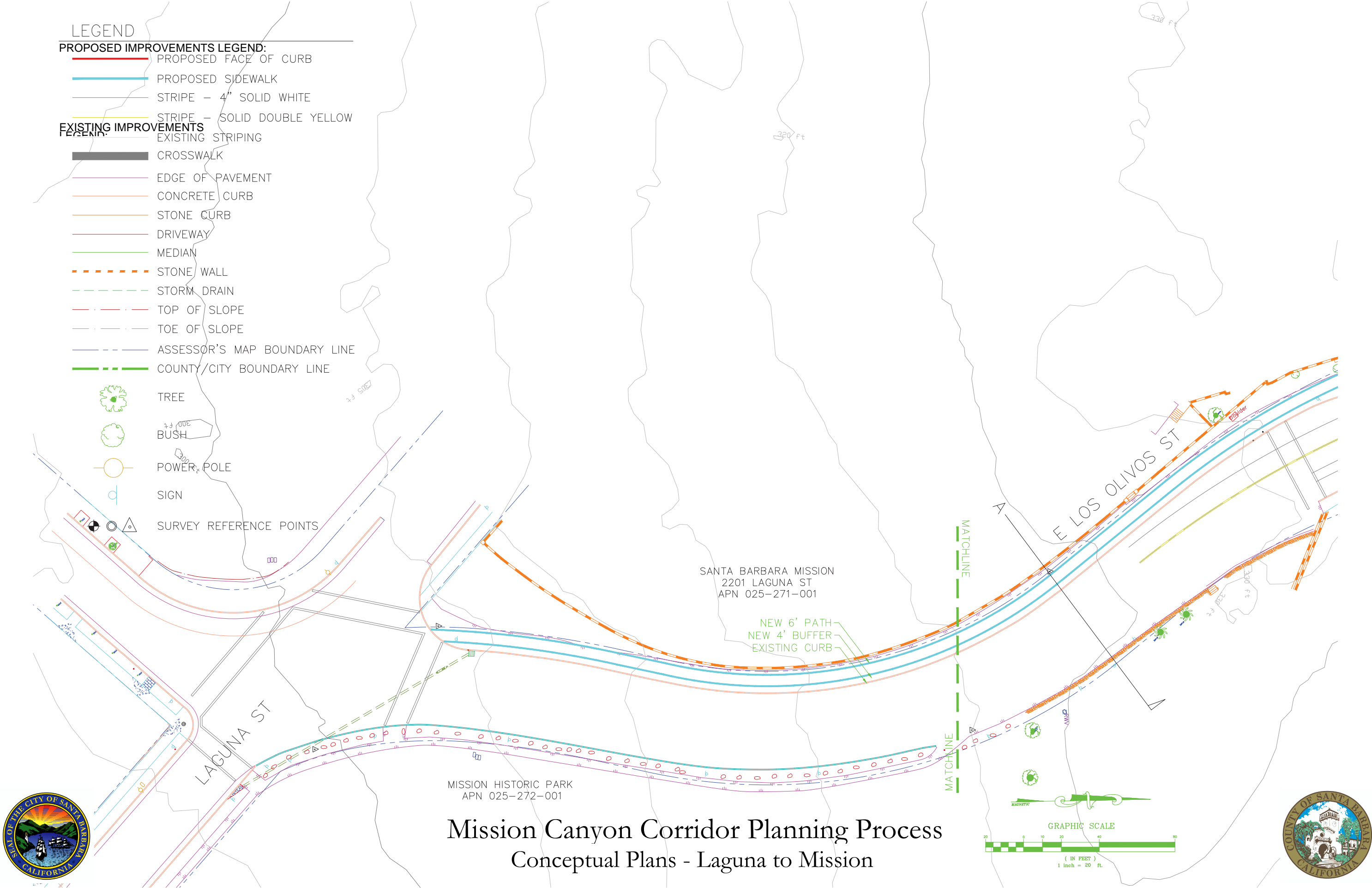
### Mission to Museum Photo Map





**B-1: Laguna to Mission**

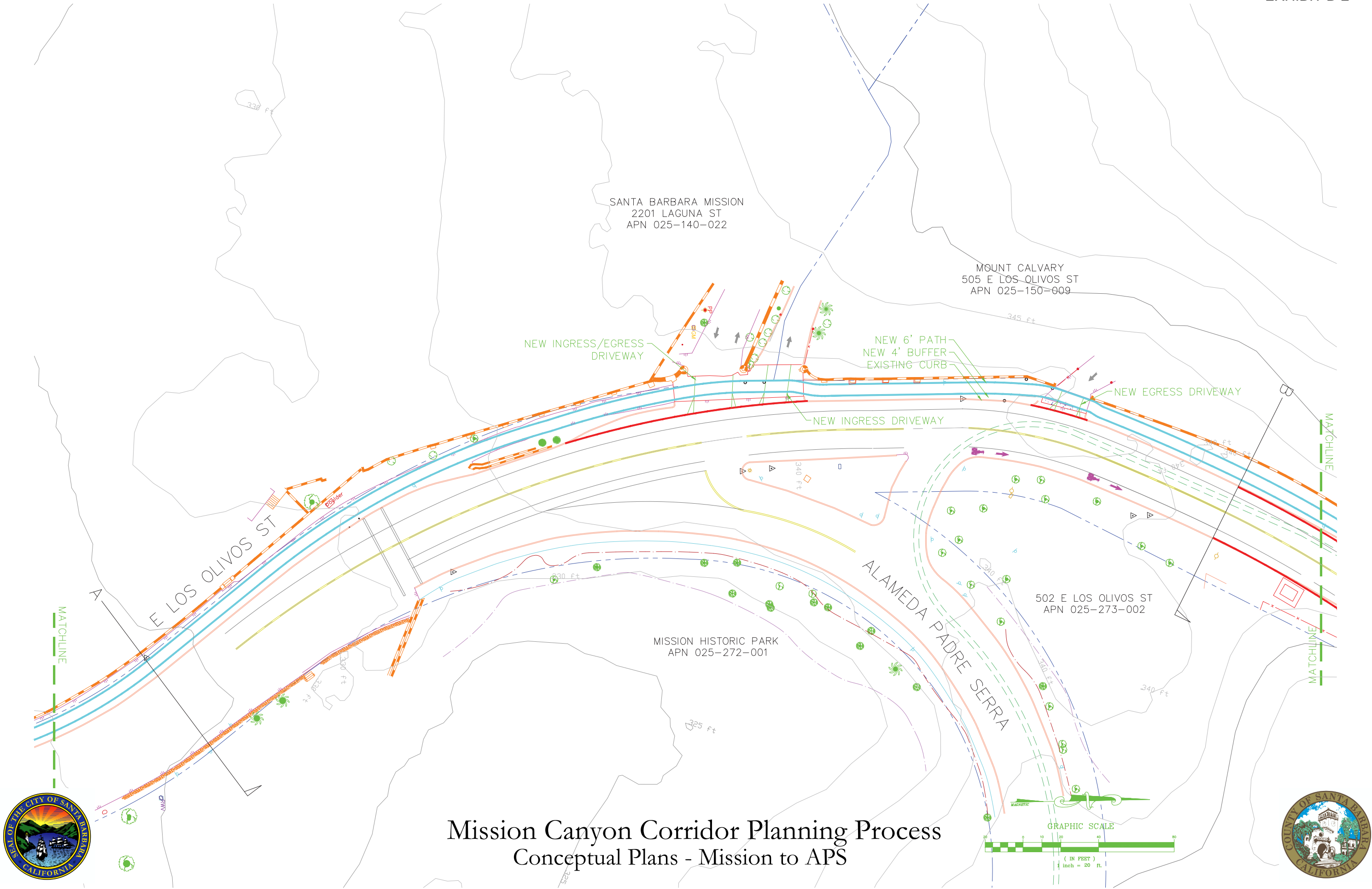




# Mission Canyon Corridor Planning Process

## Conceptual Plans - Laguna to Mission

## B-2: Mission to APS

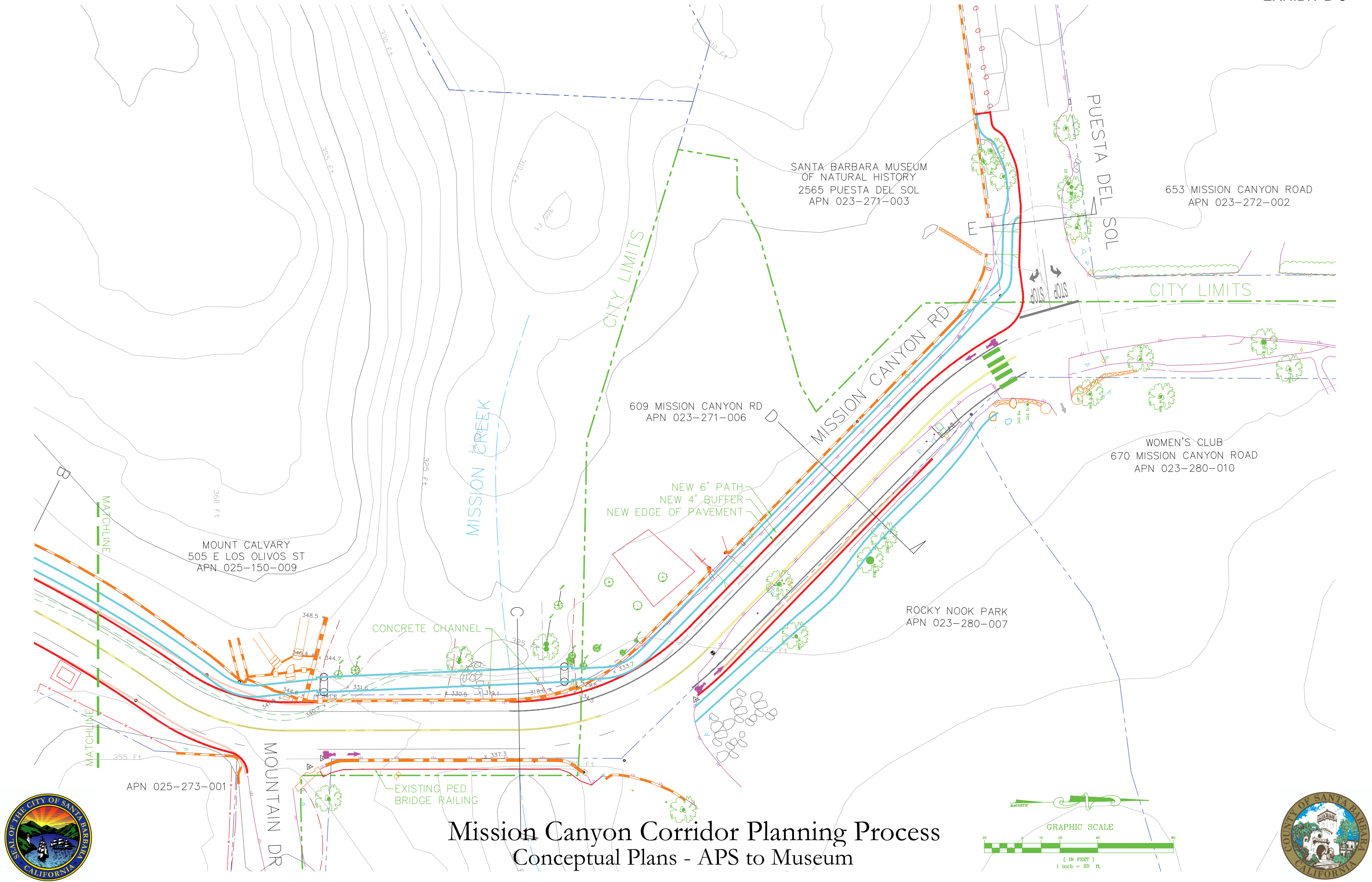


Mission Canyon Corridor Planning Process  
Conceptual Plans - Mission to APS

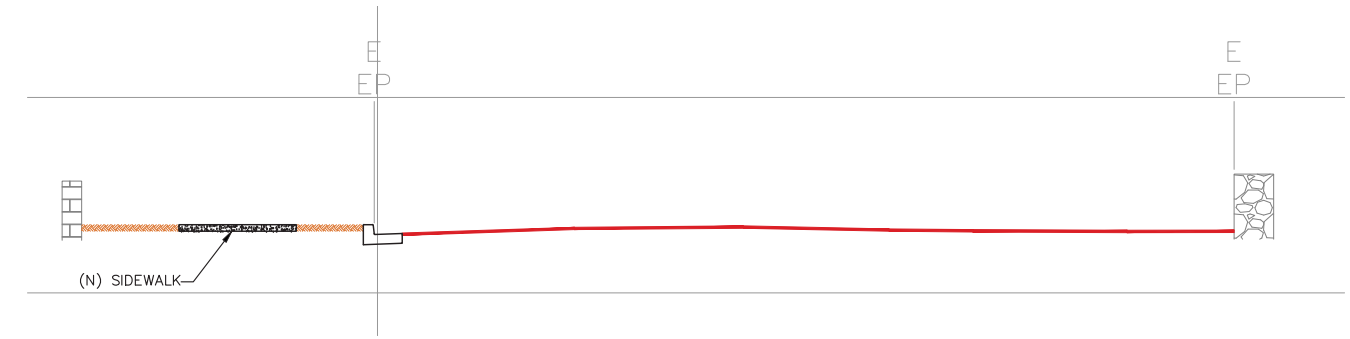


**B-3: APS to Museum**

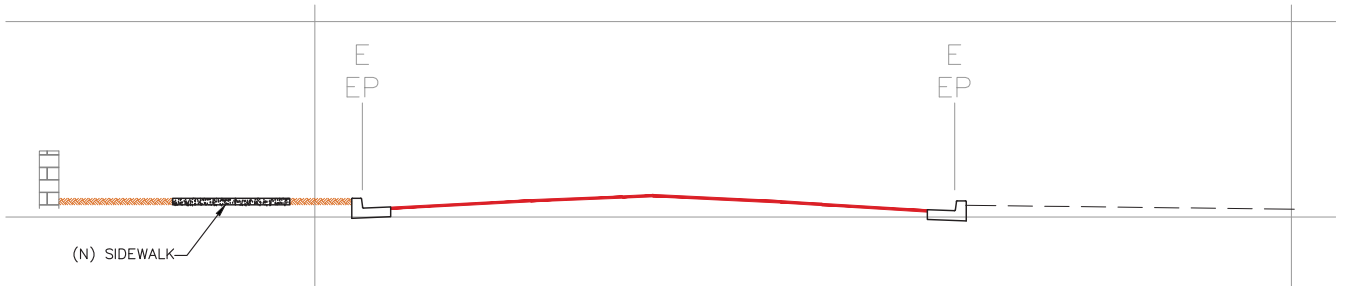




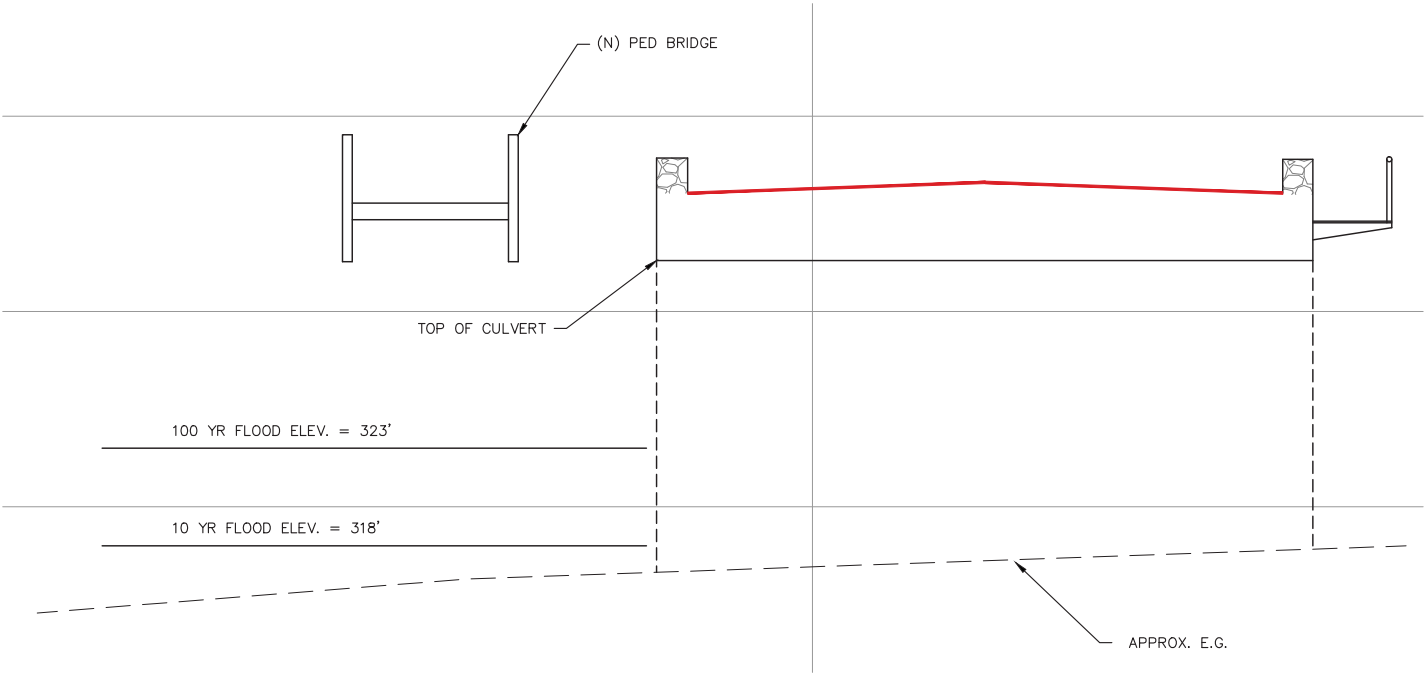
## B-4: Sections



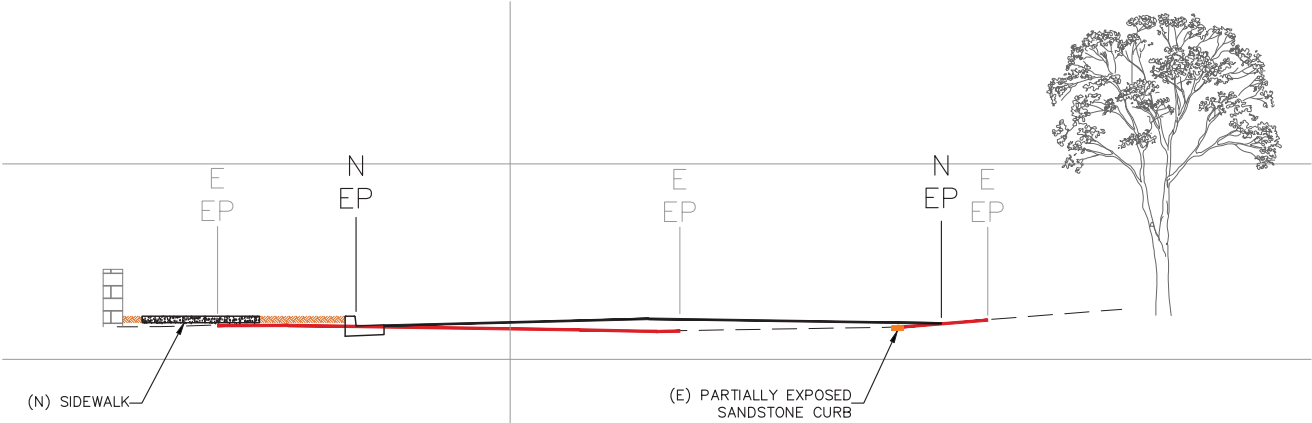
**A** MISSION CANYON AT MISSION WALL



**B** MISSION CANYON AT START OF CURB



**C** BRIDGE SECTION



**D** MISSION CANYON AT ROCK NOOK



**E** DRIVEWAY SECTION AT PUESTA DEL SOL

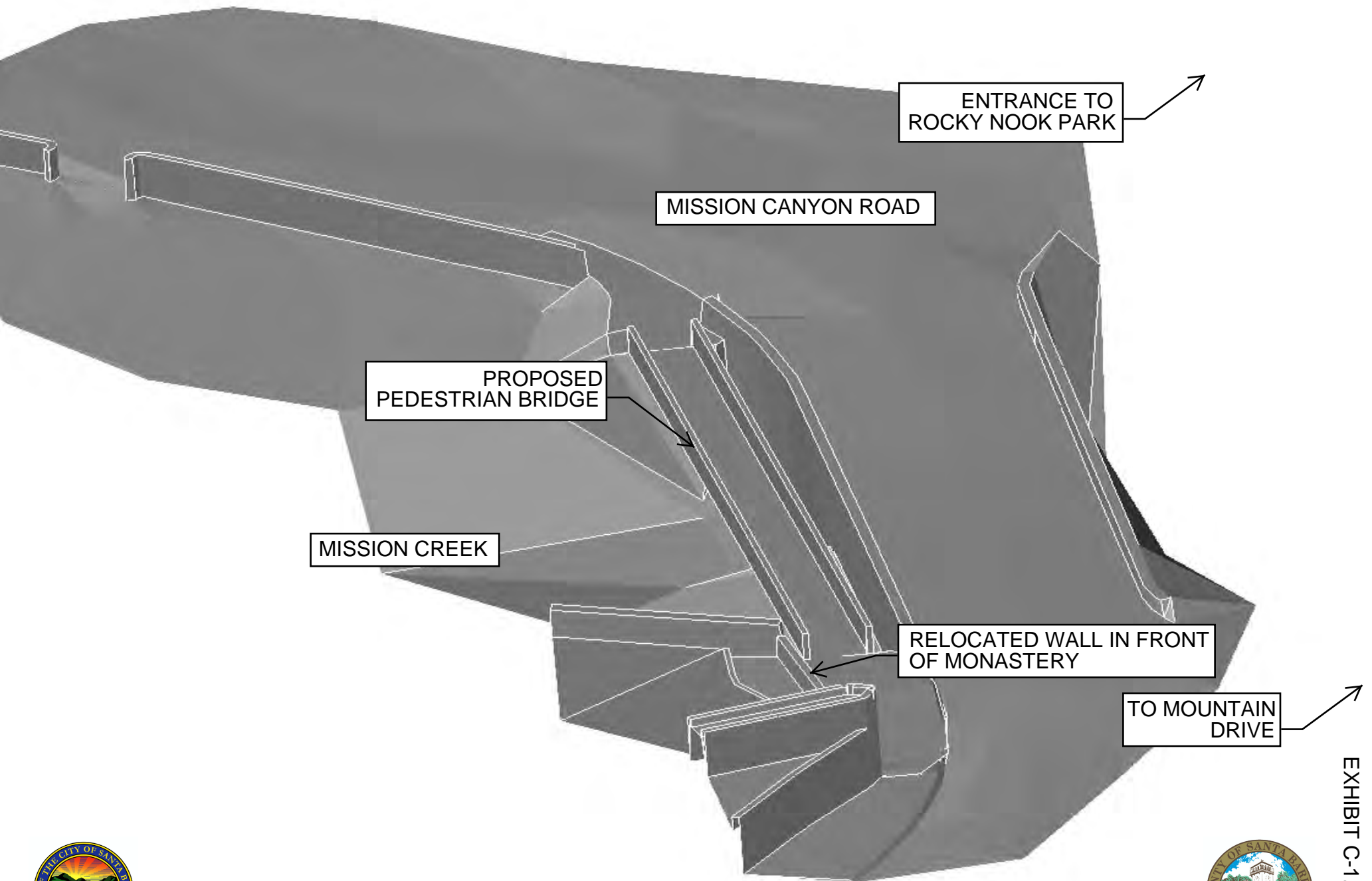


Mission Canyon Corridor Planning Process  
Conceptual Plans - Sections



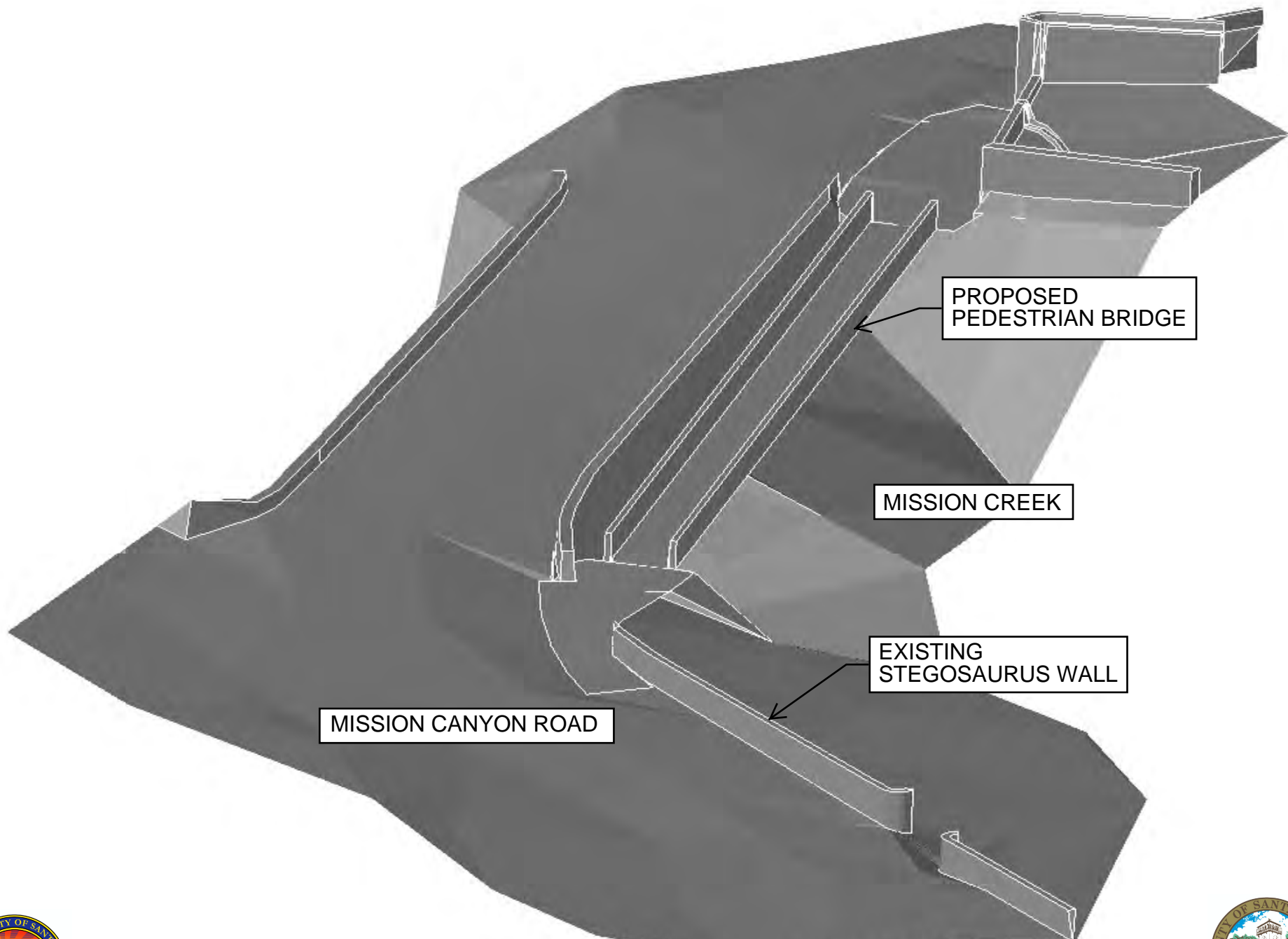
## C-1.1 – 1.2: Pedestrian Bridge Site Studies





Mission Canyon Corridor Planning Process  
Bridge Study - Looking Northerly





MISSION CANYON ROAD

PROPOSED  
PEDESTRIAN BRIDGE

MISSION CREEK

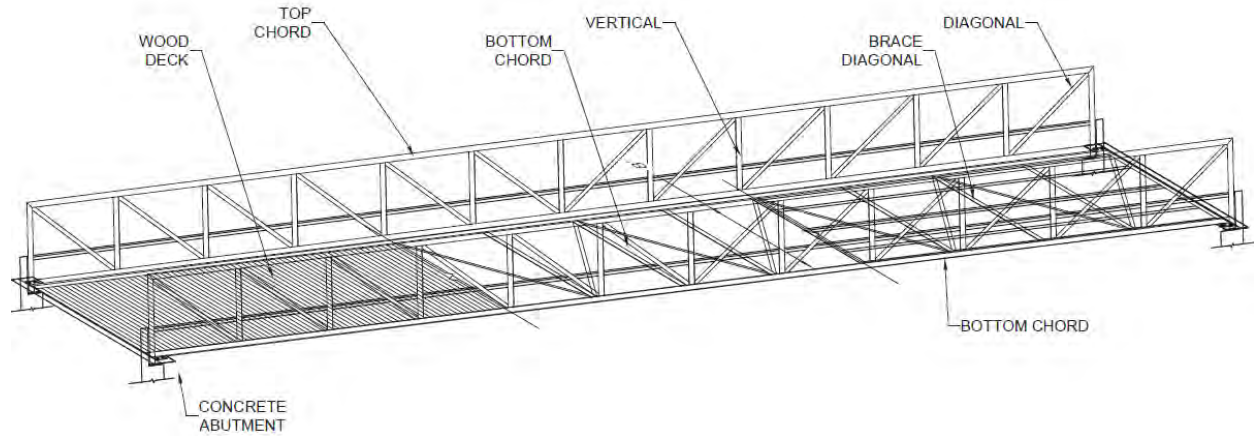
EXISTING  
STEGOSAURUS WALL



Mission Canyon Corridor Planning Process  
Bridge Study - Looking Southerly



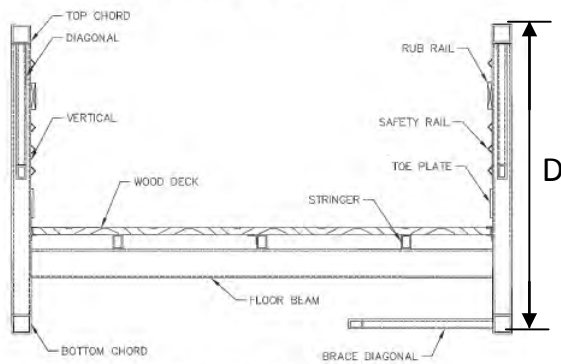
## C-2: Pedestrian Bridge Structural Details



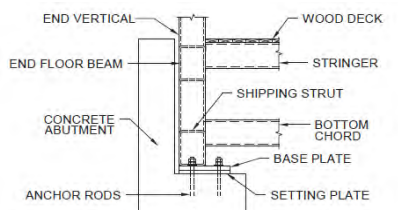
A Connector® standard truss pedestrian bridge, or equivalent, is proposed with a clear span of 125 feet from top of bank to top of bank.

### Typical Truss Styles

#### Section Views

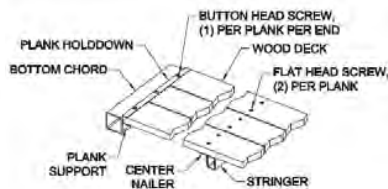


Connector® - H-Section

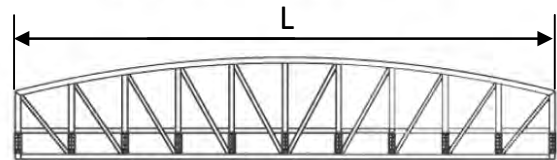


#### BEARING SIDE VIEW

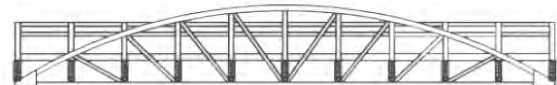
INFORMATION PROVIDED FOR REPRESENTATION ONLY.  
ACTUAL BEARING DIAGRAMS TO BE BASED ON FINAL DESIGN.



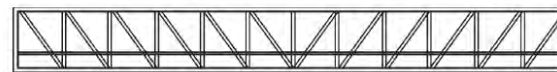
WOOD DECK DETAIL



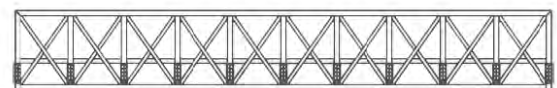
Capstone® Modified Bow Truss



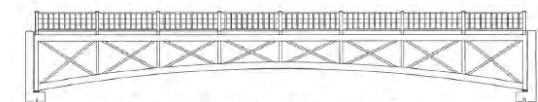
Keystone® Bow Truss



Connector® Standard Truss



Link® X-Brace Truss



Archway® Underhung Truss

### How tall will the side trusses be on my bridge?

If your bridge is a Half-Through Pony System, the truss height will be a minimum of 42" for pedestrian only traffic, a minimum of 54" for pedestrians with bicycle traffic or  $L/20$  (the bridge length divided by 20).

The proposed clear span (L) is 125 feet long; therefore the total depth (D) of the truss will be approximately 6.25 feet.







Mason St Ped Bridge



Arroyo Burro Ped Bridge (72' span, 4'3" truss, 5'6 width, vertical pickets)



## Mission Canyon Corridor Planning Process

### Pedestrian Bridge Structural Details







Westmont College Ped Bridge (129' span, 7' truss height)

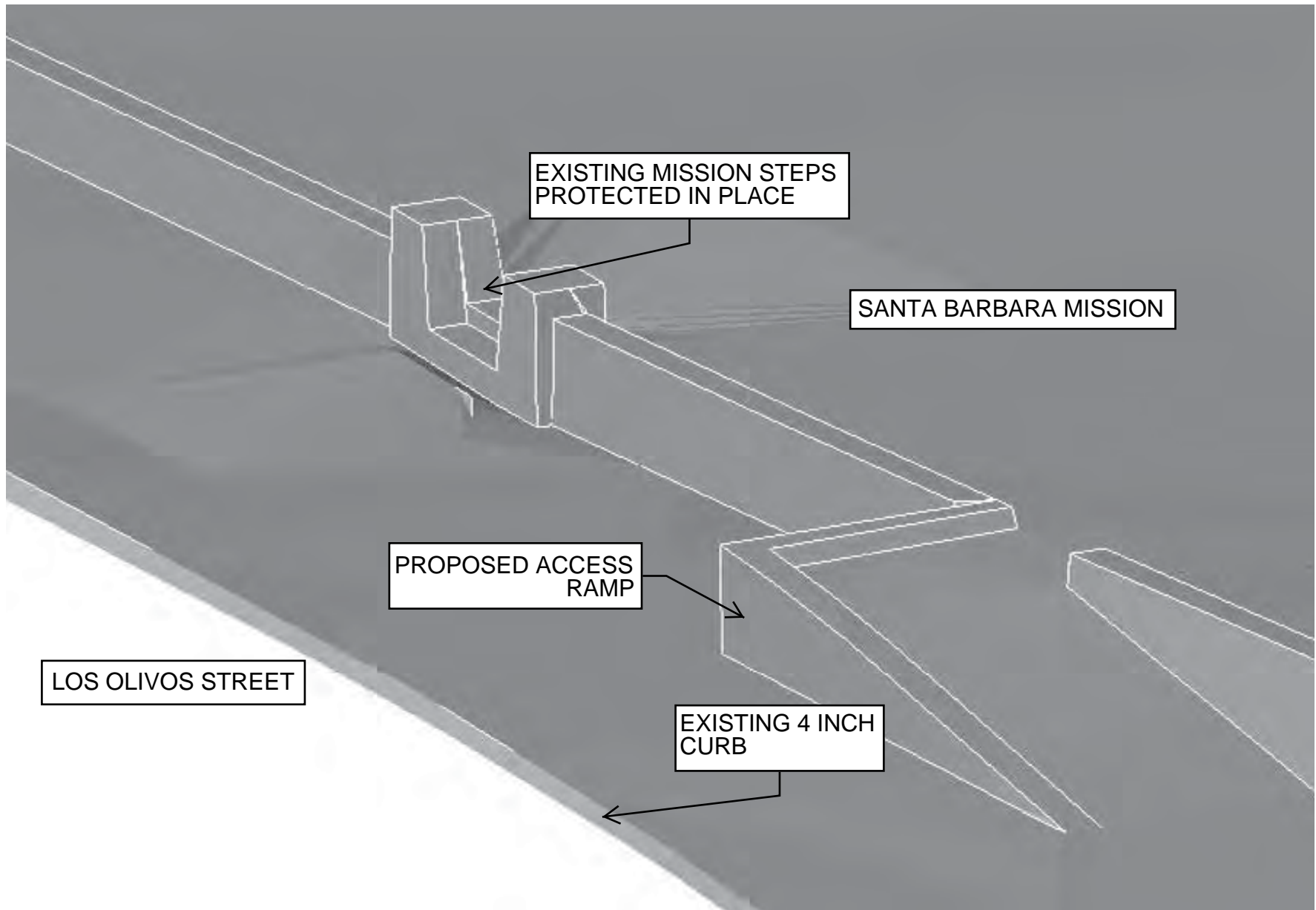


## Mission Canyon Corridor Planning Process

### Pedestrian Bridge Structural Details



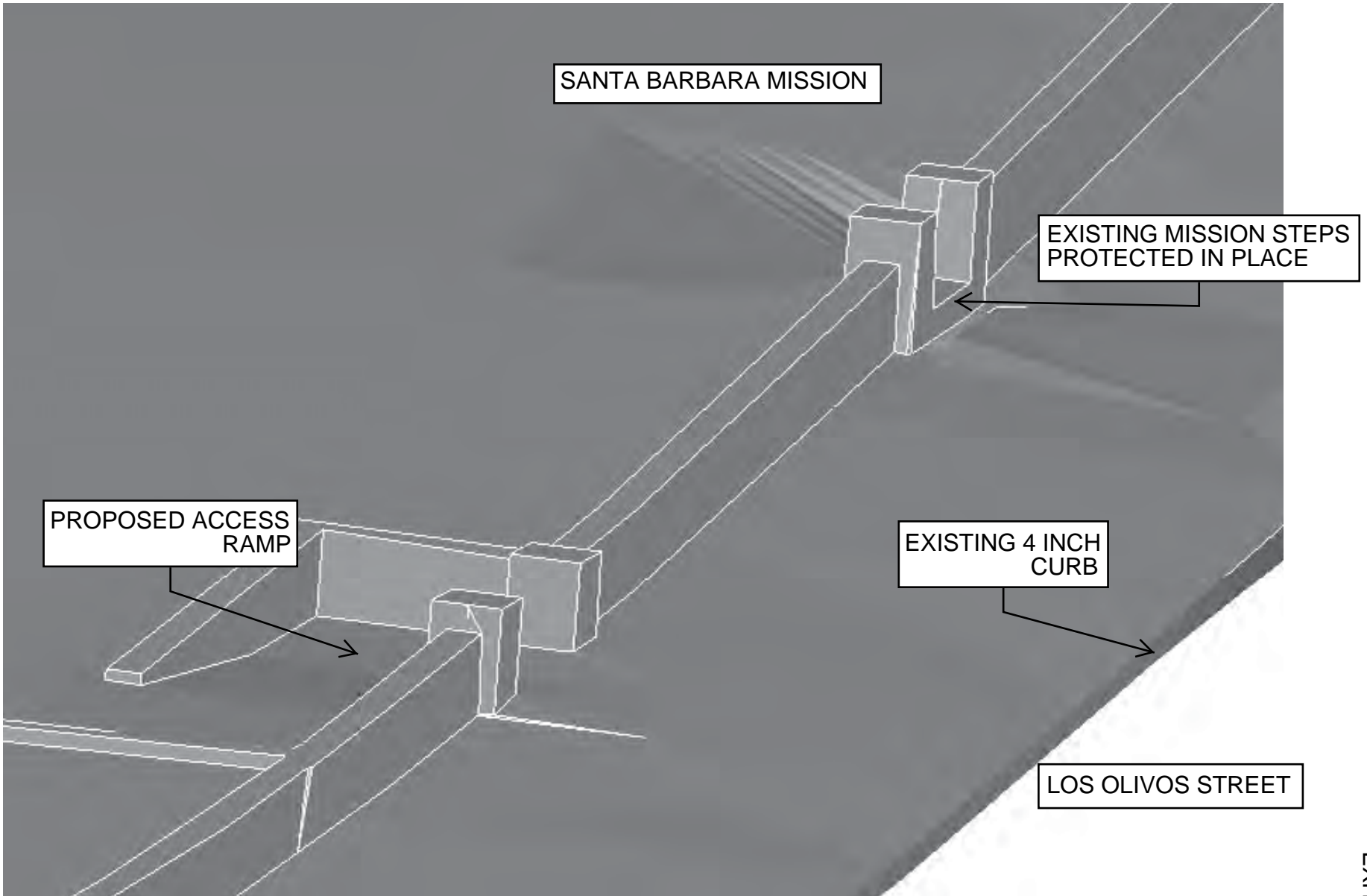
C-3.1 – 3.2: Santa Barbara Mission  
Access Ramp Studies



Mission Canyon Corridor Planning Process  
Mission Access Ramp Study - On Existing ROW



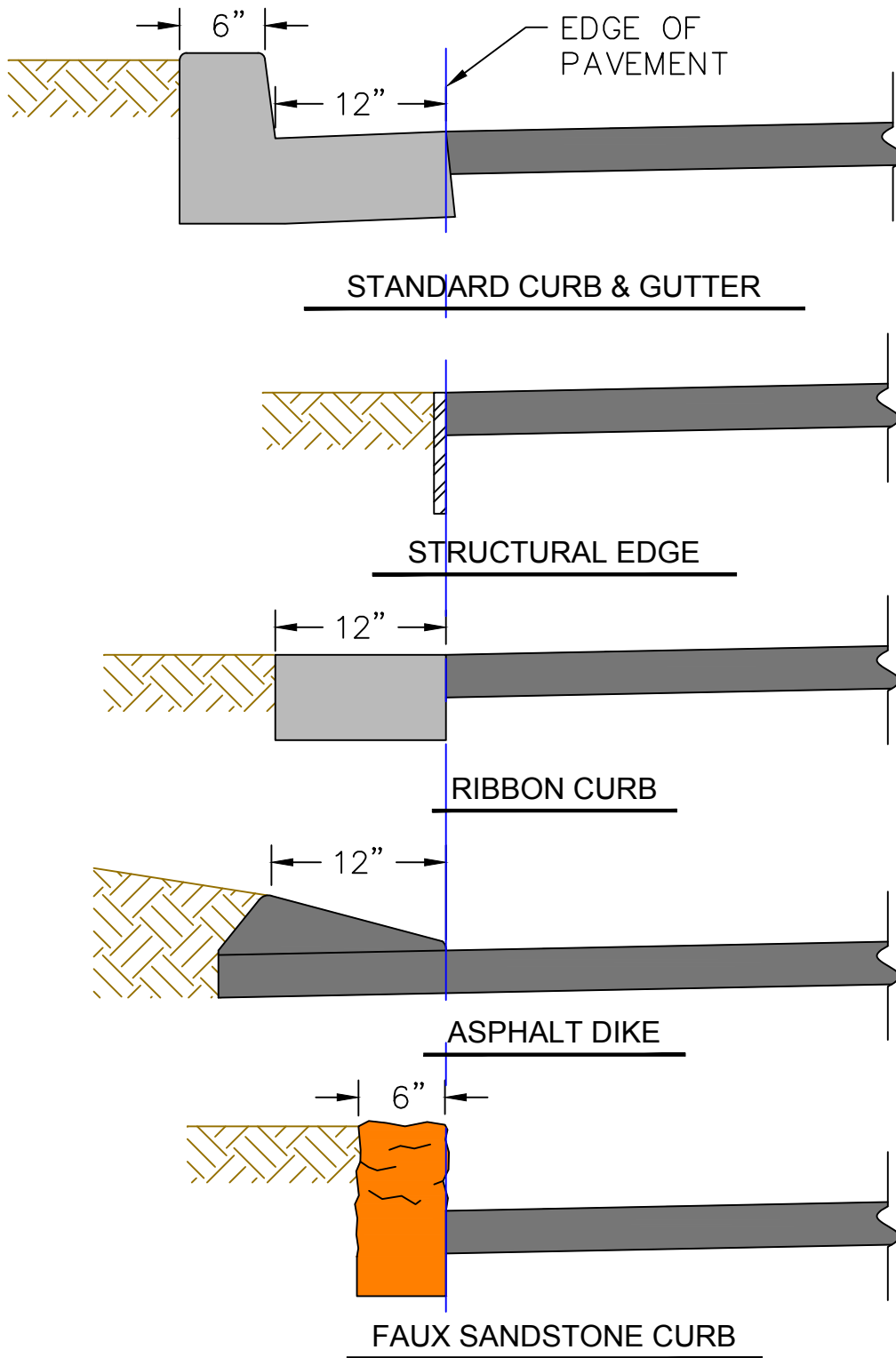




Mission Canyon Corridor Planning Process  
Mission Access Ramp Study - On Mission Property



## C-4: Sidewalk Edge Studies





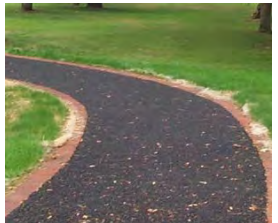




Mission Canyon Corridor Planning Process  
Sidewalk Edge Studies



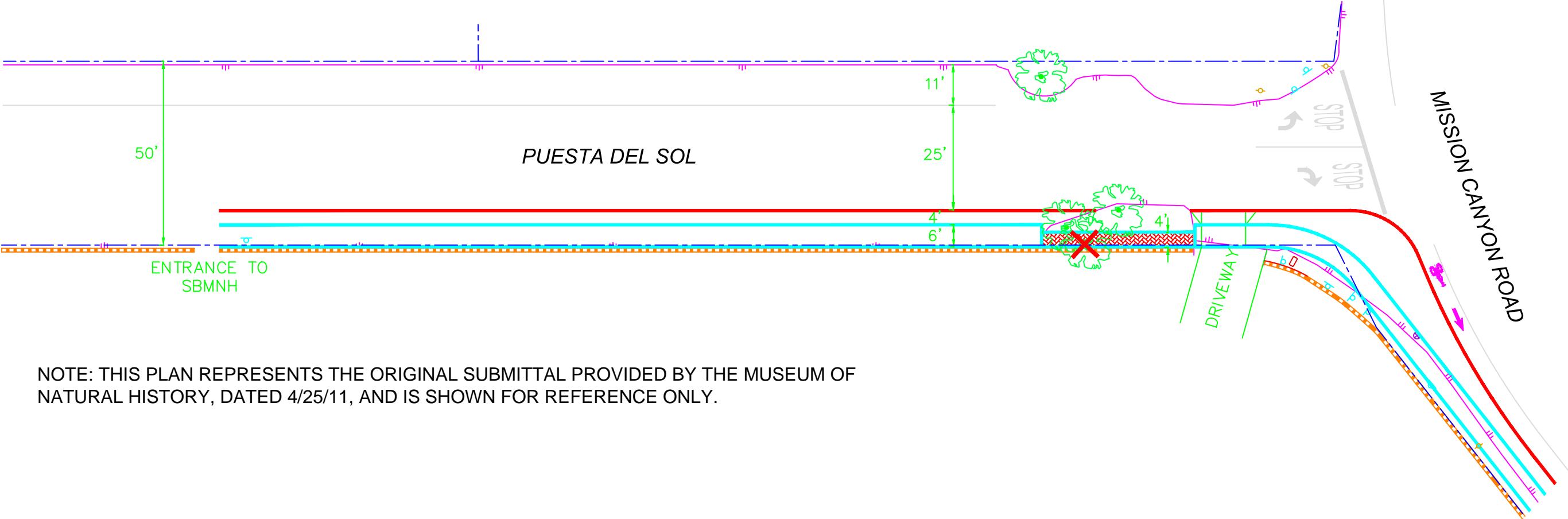
## C-5: Sidewalk Surface Studies



Surface	Estimated Cost (\$/SF)*	Opportunities	Constraints
Concrete (4") 	7 – 12	<ul style="list-style-type: none"> <li>• 25+ year design life</li> <li>• Low maintenance</li> <li>• Low rolling resistance</li> <li>• May be used on steep grades</li> <li>• Reflects heat</li> <li>• Can be colored for aesthetics</li> </ul>	<ul style="list-style-type: none"> <li>• Rigid - prone to cracking</li> <li>• Contraction joints</li> <li>• Expensive to replace</li> </ul>
Asphalt (3") 	3 – 5	<ul style="list-style-type: none"> <li>• Fairly low maintenance</li> <li>• Low rolling resistance when maintained</li> <li>• 15-20 year design life</li> <li>• Moderate up-front costs</li> <li>• Quiet/Soft/Flexible when installed</li> <li>• Joint-free when installed</li> <li>• Quick installation – minimal excavation req.</li> </ul>	<ul style="list-style-type: none"> <li>• Prone to uplift/cracking</li> <li>• Aggregate may delaminate over time</li> <li>• Absorbs heat</li> </ul>
Permeable Pavers 	15 – 20	<ul style="list-style-type: none"> <li>• 25+ year design life</li> <li>• Permeable</li> <li>• Easy to access underground utilities</li> <li>• Reduces Stormwater Permit requirements</li> <li>• High compressive strength</li> <li>• Allows for differential movement</li> </ul>	<ul style="list-style-type: none"> <li>• Higher up-front costs</li> <li>• Deeper excavation required to allow for permeable base</li> <li>• Difficult to maintain when clogged</li> </ul>
Porous Concrete (4") 	9 – 15	<ul style="list-style-type: none"> <li>• 25+ year design life</li> <li>• Permeable</li> <li>• Reduces Stormwater Permit requirements</li> <li>• Can be colored for aesthetics</li> </ul>	<ul style="list-style-type: none"> <li>• Rigid - prone to cracking</li> <li>• Difficult to install properly</li> <li>• Difficult to maintain when clogged</li> <li>• Lower compressive strength than conventional concrete</li> <li>• At end of design life, requires replacement by milling to base</li> </ul>
Porous Asphalt (4") 	3.5 – 6	<ul style="list-style-type: none"> <li>• 15-20 year design life</li> <li>• Permeable</li> <li>• Reduces Stormwater Permit requirements</li> </ul>	<ul style="list-style-type: none"> <li>• Absorbs heat</li> <li>• Difficult to install properly</li> <li>• Difficult to maintain when clogged</li> <li>• Lower compressive strength than conventional asphalt</li> <li>• At end of design life, requires replacement by milling to base</li> </ul>
Decomposed Granite 	1.5 – 2	<ul style="list-style-type: none"> <li>• Natural aesthetic</li> <li>• Cost effective</li> <li>• Easy to patch for access underground utilities</li> <li>• Minimal excavation</li> </ul>	<ul style="list-style-type: none"> <li>• Subject to rutting</li> <li>• Regular maintenance required</li> <li>• May lead to fines runoff</li> <li>• Short design life</li> <li>• More difficult for wheelchair access</li> </ul>
Stabilized Aggregate 	4 – 8	<ul style="list-style-type: none"> <li>• Natural aesthetic</li> <li>• Cost effective</li> <li>• Minimal excavation</li> </ul>	<ul style="list-style-type: none"> <li>• Aggregate may delaminate over time</li> <li>• Less environmental impact than asphalt</li> <li>• Glossy sheen may be undesirable</li> </ul>

\* Costs are for general comparison only. Costs include minimum required excavation, base, subbase, and material cost. Mobilization and other extraneous costs are not included. Actual material/installation costs may vary.

C-6.1 – 6.2: Santa Barbara Museum of  
Natural History - Puesta del Sol  
Pedestrian Options

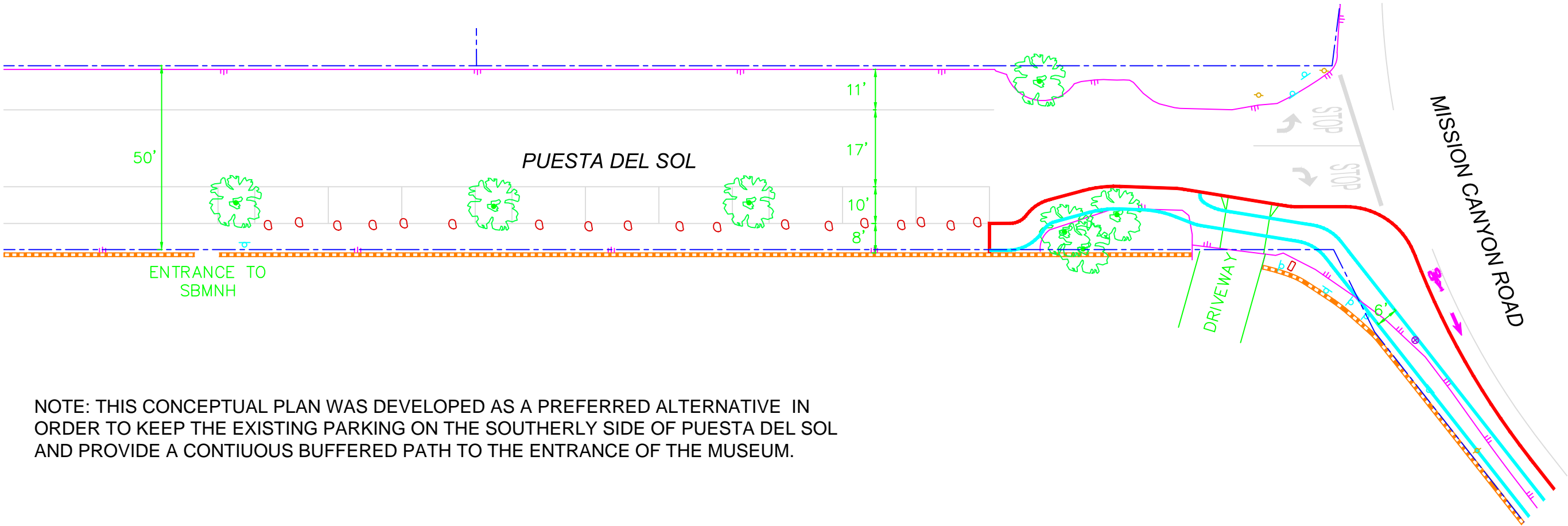


NOTE: THIS PLAN REPRESENTS THE ORIGINAL SUBMITTAL PROVIDED BY THE MUSEUM OF NATURAL HISTORY, DATED 4/25/11, AND IS SHOWN FOR REFERENCE ONLY.

PUESTA DEL SOL  
PEDESTRIAN IMPROVEMENTS  
MUSEUM OF NATURAL HISTORY ORIGINAL  
DEVELOPMENT PLAN SUBMITTAL, DATED 4/25/2011

-PRELIMINARY-  
NOT FOR CONSTRUCTION

SCALE: 1" = 30'  
SHEET 1 OF 2



NOTE: THIS CONCEPTUAL PLAN WAS DEVELOPED AS A PREFERRED ALTERNATIVE IN ORDER TO KEEP THE EXISTING PARKING ON THE SOUTHERLY SIDE OF PUESTA DEL SOL AND PROVIDE A CONTIUOUS BUFFERED PATH TO THE ENTRANCE OF THE MUSEUM.

PUESTA DEL SOL  
PEDESTRIAN IMPROVEMENTS  
PREFERRED ALTERNATIVE

-PRELIMINARY-  
NOT FOR CONSTRUCTION

SCALE: 1" = 30'  
SHEET 2 OF 2



## D-1 – 3: PHOTO PERSPECTIVES

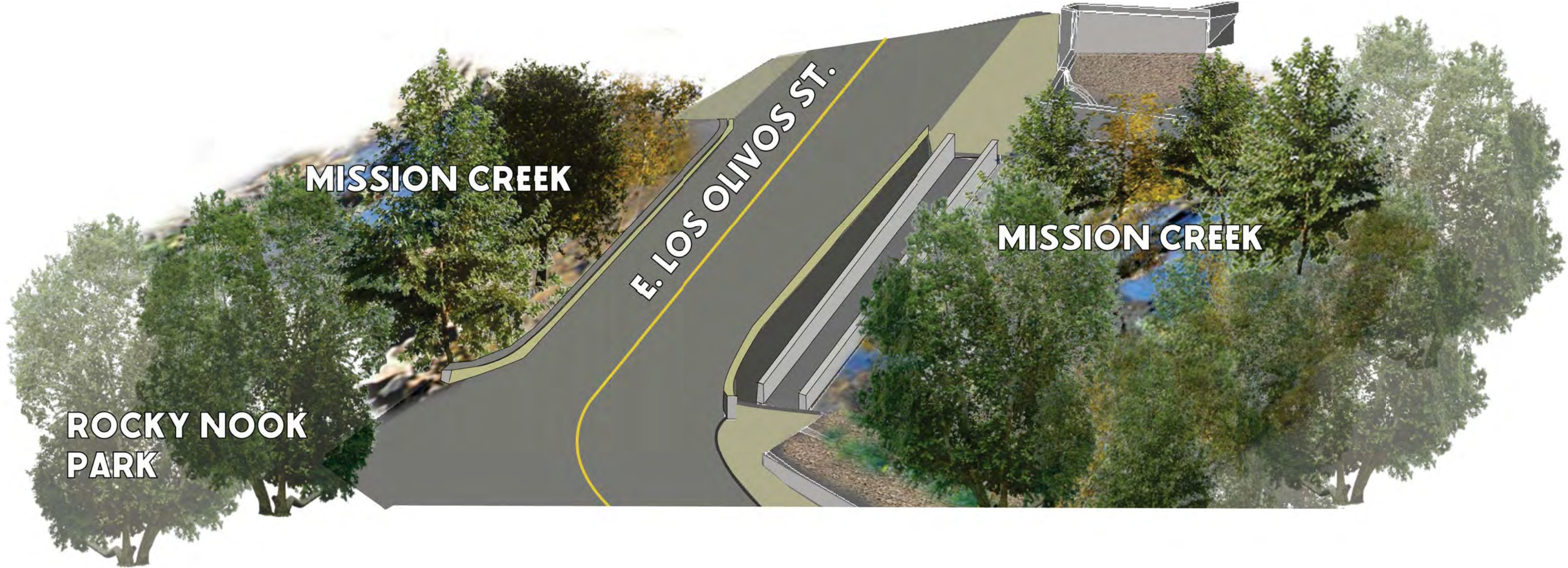




Mission Canyon Corridor Planning Process  
Mission Canyon Bridge - Street Level







Mission Canyon Corridor Planning Process  
Mission Canyon Bridge - Axonometric







Mission Canyon Corridor Planning Process  
Mountain Drive - Section





## E-1.1 – 1.3: Plans





Mission Canyon 7cfffXcfD'Ubb]b[ 'DfcWgg  
.....Laguna to Mission



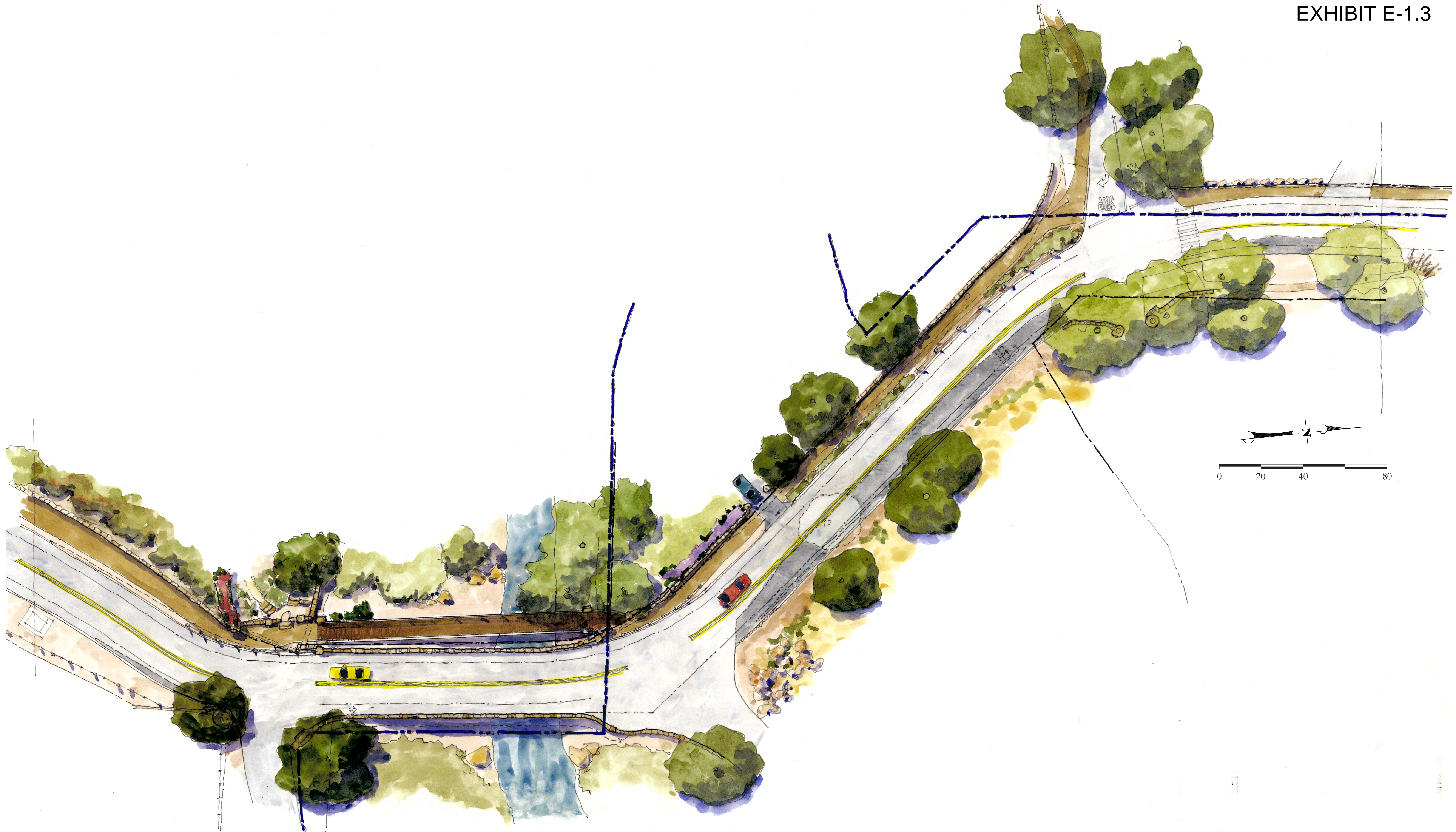




Mission Canyon 7cff]Xcf'D'Ubb]b[ 'DfcWgg  
Mission to APS







Mission Canyon 7cff[Xcf'D'Ubb]b[ 'DfcWgg  
''APS to Museum





## E-2.1 – 2.5: Sections





Mission Canyon  
at Mission Santa Barbara

0 5 10 20







Mission Canyon 7cff]Xcf'D'Ubb]b[ 'DfcWgg  
at Mountain Drive







Mission Canyon 7cff]Xcf'D'Ubb]b[ 'DfcWgg  
'''at Mission Canyon Bridge







Mission Canyon 7cff]Xcf'D`Ubb]b[ 'DfcWgg  
'''at Rocky Nook Park







Mission Canyon Safe Passage Project  
at Puesta del Sol



## F-1 – 3: FIRST PUBLIC MEETING SUMMARY

# **MISSION CANYON CORRIDOR LISTENING WORKSHOP**

Tuesday, October 29, 2013  
6:30 p.m.  
Santa Barbara Woman's Club

## **Agenda**

- |             |                     |
|-------------|---------------------|
| 6:30 – 7:00 | Refreshments        |
| 7:00 – 7:15 | Presentation        |
| 7:15 – 8:45 | Table Discussions   |
| 8:45 – 9:00 | Wrap Up and Adjourn |





Mission Canyon  
Corridor Listening  
Workshop  
October 29, 2013

# Caltrans Grant Key Steps



# Results

- Preferred concept level master plan and technical report
- Next Steps County/City Agreement





# Corridor Map



# Features



Mission Santa Barbara



Mission Historic Park



WW I Memorial Olive Grove



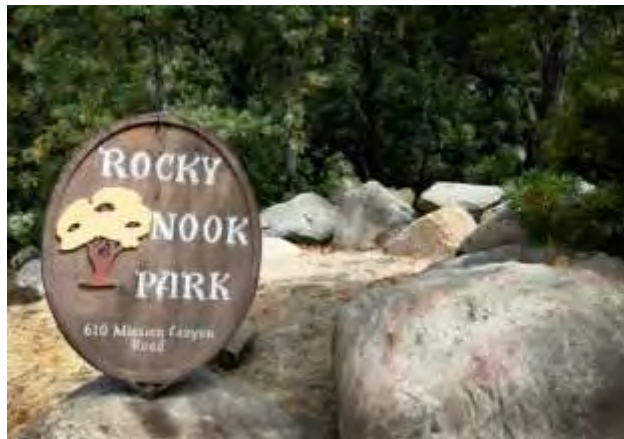
Aqueduct Wall



Historic Stone Bridge



Stone walls and mature oak trees



Rocky Nook Park



Rockwood - Santa Barbara Woman's Club



# Thanks for coming



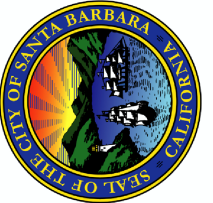
- Questions?
- Community Meeting Survey
- For more information:

[http://longrange.sbcountyplanning.org/planareas/mission\\_canyon/mc\\_multimodal.php](http://longrange.sbcountyplanning.org/planareas/mission_canyon/mc_multimodal.php)





Mission Canyon Corridor  
Planning Process  
1st Public Meeting Corridor  
Display





# G-1 – 4: SECOND PUBLIC MEETING SUMMARY



# AGENDA

## Mission Canyon Corridor Public Workshop #2 – “What the Community is Saying”

April 22, 2014

6:30 – 8:00 PM

Santa Barbara Woman's Club

**Purpose:** Receive feedback on the Listening Workshop held October 29, 2013 – what is the community saying about the Mission Canyon corridor? We will also get community feedback on initial concepts for improvements along the corridor.

Agenda Item	Discussion Topic	Estimated Time	Lead
1	<b>Welcome and Introductions</b> <ul style="list-style-type: none"> <li>➤ Staff introductions</li> <li>➤ Workshop purpose and format</li> </ul>	5 min	Rosie Dyste, County Planning & Development
2	<b>Workshop #1 Recap – What is the community saying about the corridor</b> <ul style="list-style-type: none"> <li>➤ What's important?</li> <li>➤ What's not working?</li> <li>➤ What needs to be fixed?</li> <li>➤ What needs to be left alone?</li> </ul>	15 min	Rosie Dyste
3	<b>What's “doable”?</b> <ul style="list-style-type: none"> <li>➤ Review and get community feedback of possible improvements.</li> </ul>	40 min	Robert Dayton, City Public Works Matt Dobberteen, County Public Works
4	<b>What should be included on the long-term plan?</b> <ul style="list-style-type: none"> <li>➤ Review and feedback of other community desires</li> </ul>	20 min	Matt Dobberteen
5	<b>Next Steps</b> <ul style="list-style-type: none"> <li>➤ Upcoming opportunities to participate</li> <li>➤ Community Meeting Survey</li> </ul>	10 min	Rosie Dyste

Questions or comments about the Mission Park to Mission Canyon Multimodal Improvements Project may be directed to Rosie Dyste at (805) 568-3532, or [rdyste@co.santa-barbara.ca.us](mailto:rdyste@co.santa-barbara.ca.us).

**Further information may be obtained on the following website:**

[http://longrange.sbcountyplanning.org/planareas/mission\\_canyon/mc\\_multimodal.php](http://longrange.sbcountyplanning.org/planareas/mission_canyon/mc_multimodal.php)

) h



## Mission Canyon Corridor Planning Process



# What's Important?



# 1 History



## 2 Views and Aesthetics





# 3 Ecological Resources

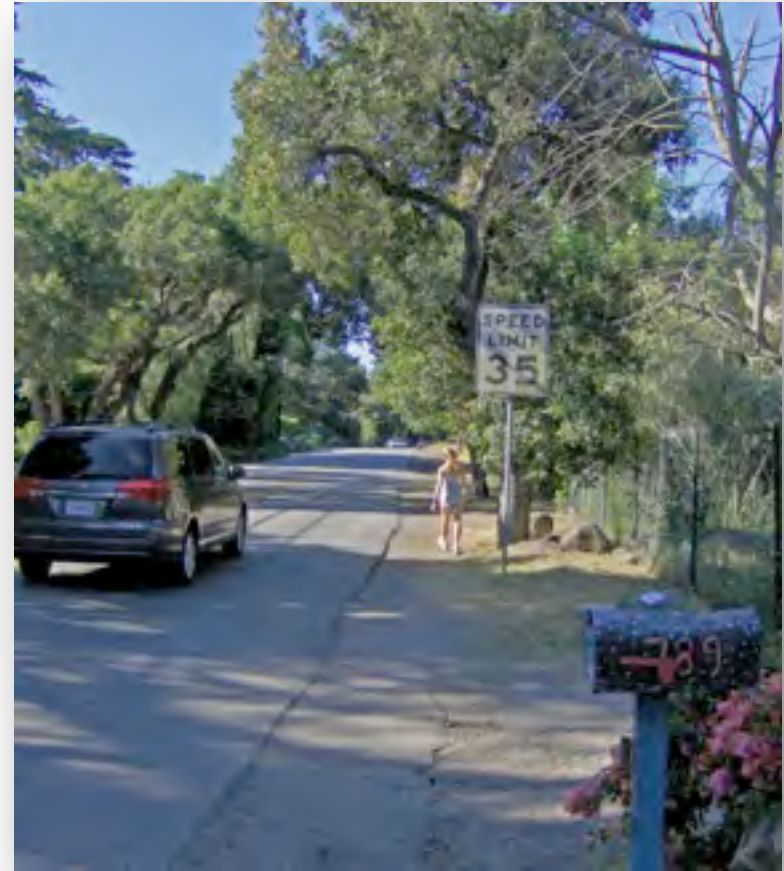




# 4 Destinations



# 5 Connectivity



What's not working well?



# 1 Automobiles and Intersections



## 2 Pedestrian Passage



# 3 Bicyclist Passage





# 4 Signage



# 5 Utility Poles



What needs to be fixed?



# 1 Walkways



## 2 Safety



# 3 Roads and Intersections





# 4 Bicycle Safety



# 5 Signage/Utility Poles



What needs to be left alone?



# 1 Bridge



## 2 Stone Walls





# 3 Historic and Archaeological Resources





# 4 Ecological Resources



# 5 Rural Quality







## Mission Canyon Corridor Workshop #2 April 22, 2014

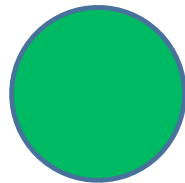


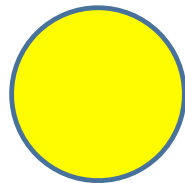
# What is “doable”?

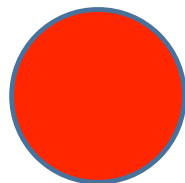
- City and County Public Works Presentation
- Dot Vote
- Other ways to comment



# Dot Consensus

 = "I approve."

 = "Not perfect, but I can live with it."

 = "I can't support it."



Mission Canyon Corridor Planning Process  
Laguna to Mission







EXHIBIT G-3.5

Wednesday, April 23, 14



Mission Canyon Corridor Planning Process  
Mission to APS





EXHIBIT G-3.7

Wednesday, April 23, 14





EXHIBIT G-3.8

Wednesday, April 23, 14



6 FT 4 FT 5 FT 10 FT 10 FT 5 FT

EXHIBIT G-3.9









EXHIBIT G-3.11

















EXHIBIT G-3.15

Wednesday, April 23, 14





EXHIBIT G-3.16

Wednesday, April 23, 14







Mission Canyon Corridor Planning Process  
APS to Museum













EXHIBIT G-3.21

Wednesday, April 23, 14









EXHIBIT G-3.23

Wednesday, April 23, 14





EXHIBIT G-3.24

Wednesday, April 23, 14













Wednesday, April 23, 14





















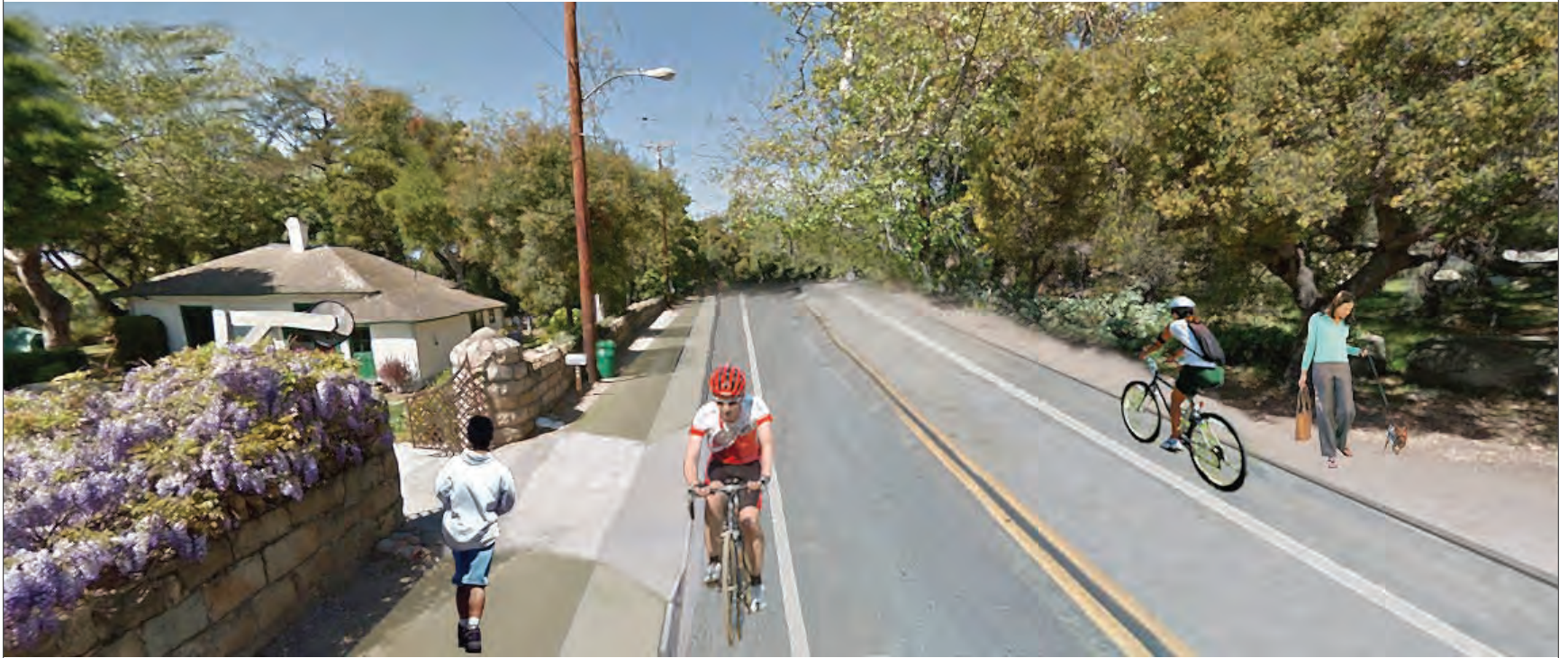




EXHIBIT G-3.34

Wednesday, April 23, 14





Wednesday, April 23, 14







# Mission Canyon Corridor Planning Process

## 2nd Public Meeting Corridor Display





## H-1: COST ESTIMATE



Mission Canyon Corridor Multimodal Improvement Project					
DETAILED ENGINEER'S ESTIMATE OF CONSTRUCTION COSTS					
Estimate Date: 11/19/14					
Item #	Item Description	Quantity	Unit of Measure	Engineer's Estimate	
				Unit Price	Item Total
<b>A</b>	<b>GENERAL</b>				
1	MOBILIZATION	1	LS	\$50,000.00	\$50,000.00
2	TRAFFIC CONTROL	1	LS	\$25,000.00	\$25,000.00
3	CLEARING & GRUBBING	1	LS	\$15,000.00	\$15,000.00
4	SWPPP	1	LS	\$15,000.00	\$15,000.00
5	SIGNAGE AND STRIPING	1	LS	\$25,000.00	\$25,000.00
6	UTILITY RELOCATION	1	LS	\$35,000.00	\$35,000.00
	<b>SUBTOTAL</b>				<b>\$165,000.00</b>
<b>B</b>	<b>MISSION TO PEDESTRIAN BRIDGE</b>				
1	HARDSCAPE REMOVAL	10000	SQFT	\$3.50	\$35,000.00
2	CRUSHED AGGREGATE BASE	250	TN	\$60.00	\$15,000.00
3	4" THICK PCC SIDEWALK	7000	SQFT	\$15.00	\$105,000.00
4	6" SANDSTONE CURB & GUTTER	580	LF	\$120.00	\$69,600.00
5	8" THICK PCC RESIDENTIAL DRIVEWAY	820	SQFT	\$24.00	\$19,680.00
6	ASPHALT CONCRETE CONFORM	30	TN	\$200.00	\$6,000.00
7	SLURRY SEAL (TYPE II)	5700	SF	\$1.20	\$6,840.00
8	PARKWAY ENHANCEMENTS	3500	SF	\$8.00	\$28,000.00
	<b>SUBTOTAL</b>				<b>\$285,120.00</b>
<b>C</b>	<b>PEDESTRIAN BRIDGE CONSTRUCTION</b>				
1	STRUCTURE EXCAVATION AND ROUGH GRADING	1	LS	\$40,000.00	\$40,000.00
2	STRUCTURE BACKFILL (BRIDGE)	1	LS	\$10,000.00	\$10,000.00
3	RELOCATE STONE WALL - AT MONASTERY	1	LS	\$30,000.00	\$30,000.00
4	RELOCATE STONE WALL - AT KAY PROPERTY	1	LS	\$20,000.00	\$20,000.00
5	CIDH PILE 30" DIAMETER	320	LF	\$220.00	\$70,400.00
6	REINFORCED CONCRETE GRADE BEAM ABUTMENT	2	EA	\$15,000.00	\$30,000.00
7	WING WALL	60	LF	\$480.00	\$28,800.00
8	8' X 125' PREFABRICATED BRIDGE	1	EA	\$250,000.00	\$250,000.00
9	BRIDGE DECKING	1000	SF	\$12.00	\$12,000.00
10	CREEK BANK PROTECTION AND REHABILITATION	1	LS	\$50,000.00	\$50,000.00
11	TREE REMOVAL	1	LS	\$10,000.00	\$10,000.00
12	LANDSCAPE MAINTENANCE	24	MO	\$400.00	\$9,600.00
13	IRRIGATION SYSTEM MATERIALS AND INSTALLATION	1	LS	\$6,000.00	\$6,000.00
	<b>SUBTOTAL</b>				<b>\$566,800.00</b>
<b>D</b>	<b>PEDESTRIAN BRIDGE TO LAS ENCINAS*</b>				
1	HARDSCAPE REMOVAL	25000	SQFT	\$3.50	\$87,500.00
2	CRUSHED AGGREGATE BASE	600	TN	\$60.00	\$36,000.00
3	4" THICK PCC SIDEWALK	2400	SQFT	\$15.00	\$36,000.00
4	SIDEWALK ACCESS RAMP	3	EA	\$10,000.00	\$30,000.00
5	6" SANDSTONE CURB & GUTTER	1140	LF	\$120.00	\$136,800.00
6	8" THICK PCC RESIDENTIAL DRIVEWAY	330	SQFT	\$24.00	\$7,920.00
7	DRAINAGE IMPROVEMENTS	1	LS	\$20,000.00	\$20,000.00
8	ROADWAY EXCAVATION AND GRADING	1	LS	\$40,000.00	\$40,000.00
9	ASPHALT CONCRETE PAVEMENT (4" THICK)	640	TN	\$200.00	\$128,000.00
10	SLURRY SEAL (TYPE II)	8300	SF	\$1.20	\$9,960.00
11	PARKWAY ENHANCEMENTS	1200	SF	\$8.00	\$9,600.00
	<b>SUBTOTAL</b>				<b>\$541,780.00</b>
<b>E</b>	<b>Sub-Total Estimated Construction Cost</b>				<b>\$1,558,700.00</b>
	35% Contingency (Includes Change Order, Scope, and Inflation Allowances)				\$545,545.00
<b>F</b>	<b>Total Estimated Construction Cost (Rounded to nearest \$1,000)</b>				<b>\$2,105,000.00</b>
	Design/Construction Management/Administrative Cost (30%)				\$631,500.00
<b>Total Estimated Project Cost</b>					<b>\$2,736,500.00</b>
*To easterly edge of Santa Barbara Museum of Natural History frontage. Assumes frontage of Museum will be improved by Museum.					

I-1.1 – 1.3: Left Turn at Las Encinas –  
Letter to Rosie Dyste (June 5, 2014)





# City of Santa Barbara

Public Works Department

www.SantaBarbaraCA.gov

June 5, 2014

**Main Office**

630 Garden Street  
P.O. Box 1990  
Santa Barbara, CA  
93102-1990

**Administration**

Tel: 805.564.5377  
Fax: 805.897.2613

**Engineering**

Tel: 805.564.5363  
Fax: 805.564.5467

**Facilities**

Tel: 805.564.5415  
Fax: 805.897.2577

**Street Maintenance**

Tel: 805.564.5413  
Fax: 805.897.1991

**Transportation**

Tel: 805.564.5385  
Fax: 805.564.5467

**Water Resources**

Tel: 805.564.5387  
Fax: 805.897.2613

Ms. Rosie Dyste, Senior Planner  
Long Range Planning  
Santa Barbara County Planning and Development  
123 E. Anapamu Street  
Santa Barbara, CA 93101

**SUBJECT:** Mission Canyon Corridor Community Process – Left Turn at Las Encinas

Dear Ms. Dyste:

For your information, here are the drawings we drafted for County Engineering staff regarding the idea to provide a new left turn lane on Mission Canyon Road at Las Encinas Road

On April 30, 2014, Derrick Bailey, Supervising City Transportation Engineer, and Andrew Grubb, City Project Engineer, met with Gary Smart and Chris Sneddon, County Traffic Engineers, at the intersection of Mission Canyon Road and Puesta del Sol, regarding the proposed Mission Canyon Corridor Multimodal Project. Several items were discussed regarding the realignment of Mission Canyon Road between the bridge and Las Encinas, including the potential for adding a left turn lane at Las Encinas, per recent public inquiries.

Since the City has been drafting alternative roadway realignments as part of the Corridor Project, Gary Smart requested that the City draft an alternative for a left turn lane at Las Encinas (see enclosure). Two alternatives are attached: 1) Installation of a left turn lane at Las Encinas, and 2) matching the realigned portion of Mission Canyon Road in front of Rocky Nook Park to the existing striping. Both alternatives match the existing turning radius of 198' (or 25 MPH) at Puesta del Sol. Please note that these exhibits are for conceptual use only and do not include potential impacts to existing improvements (e.g. driveways, utilities, etc.).

Should you or Gary Smart have any questions about the enclosed drawings, please contact Derrick Bailey, Supervising City Transportation Engineer at 805-564-5544.

Sincerely,

Pat Kelly  
Assistant Public Works Director/City Engineer

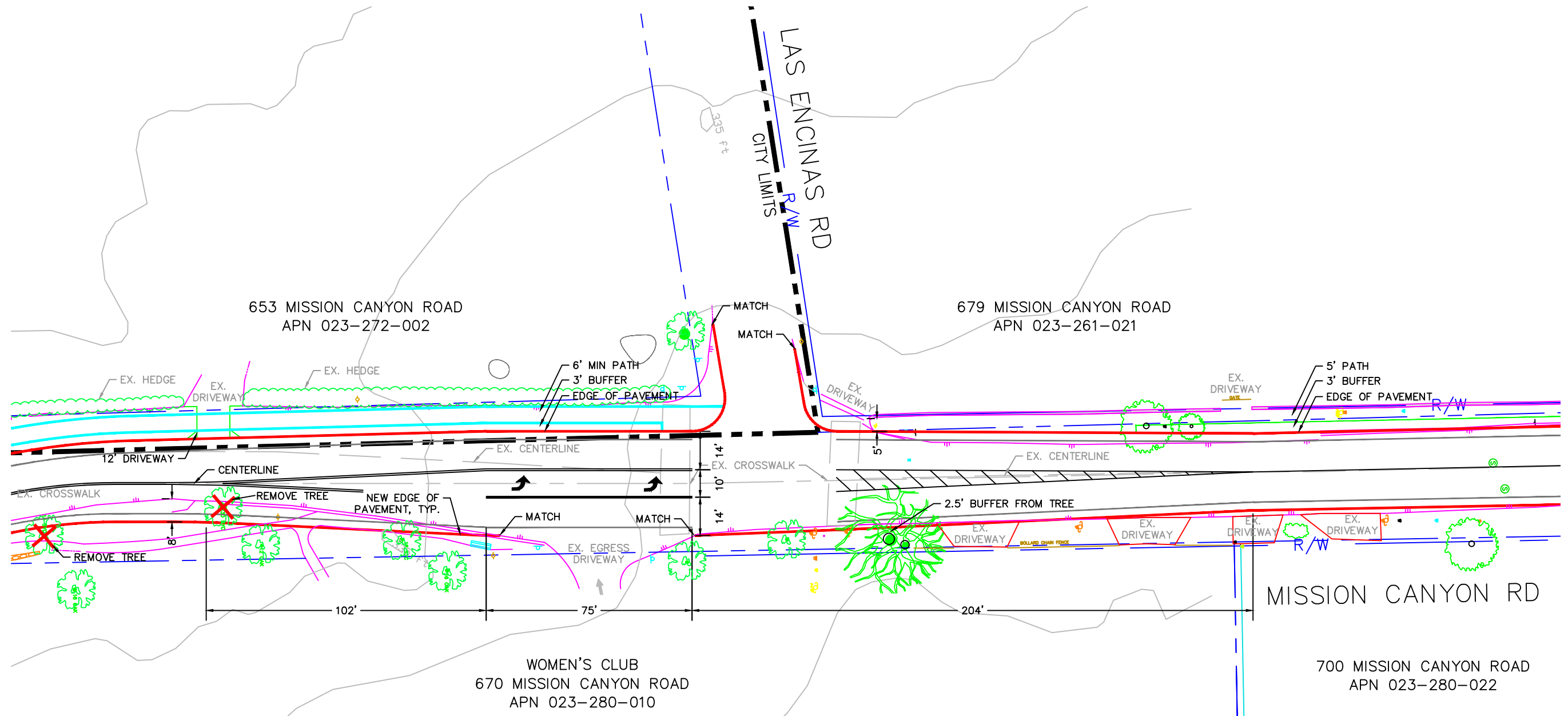
PK/sk

Enclosure: Mission Canyon Road at Las Encinas Left Turn Alternatives (2-pages)

cc: Derrick Bailey, Supervising City Transportation Engineer  
Gary Smart, County Transportation Engineer, County of Santa Barbara, 123 E.  
Anapamu St., Santa Barbara, CA 93101

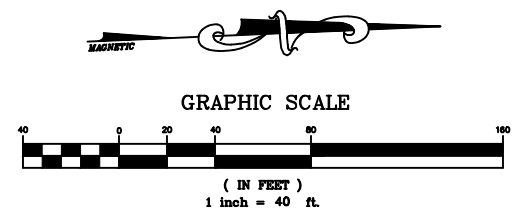
## GENERAL NOTES:

- 1) THIS PLAN IS CONCEPTUAL ONLY. ALL DIMENSIONS SHOWN SHOULD BE FIELD VERIFIED.
- 2) EXISTING IMPROVEMENTS (I.E. DRIVEWAYS, UTILITIES, ETC.) WERE NOT INCLUDED IN THE SCOPE OF THIS CONCEPTUAL DESIGN.
- 3) EXISTING SIGNAGE SHALL BE RELOCATED AS NECESSARY PER THE CITY/COUNTY TRAFFIC ENGINEER.



## MISSION CANYON ROAD AT LAS ENCINAS

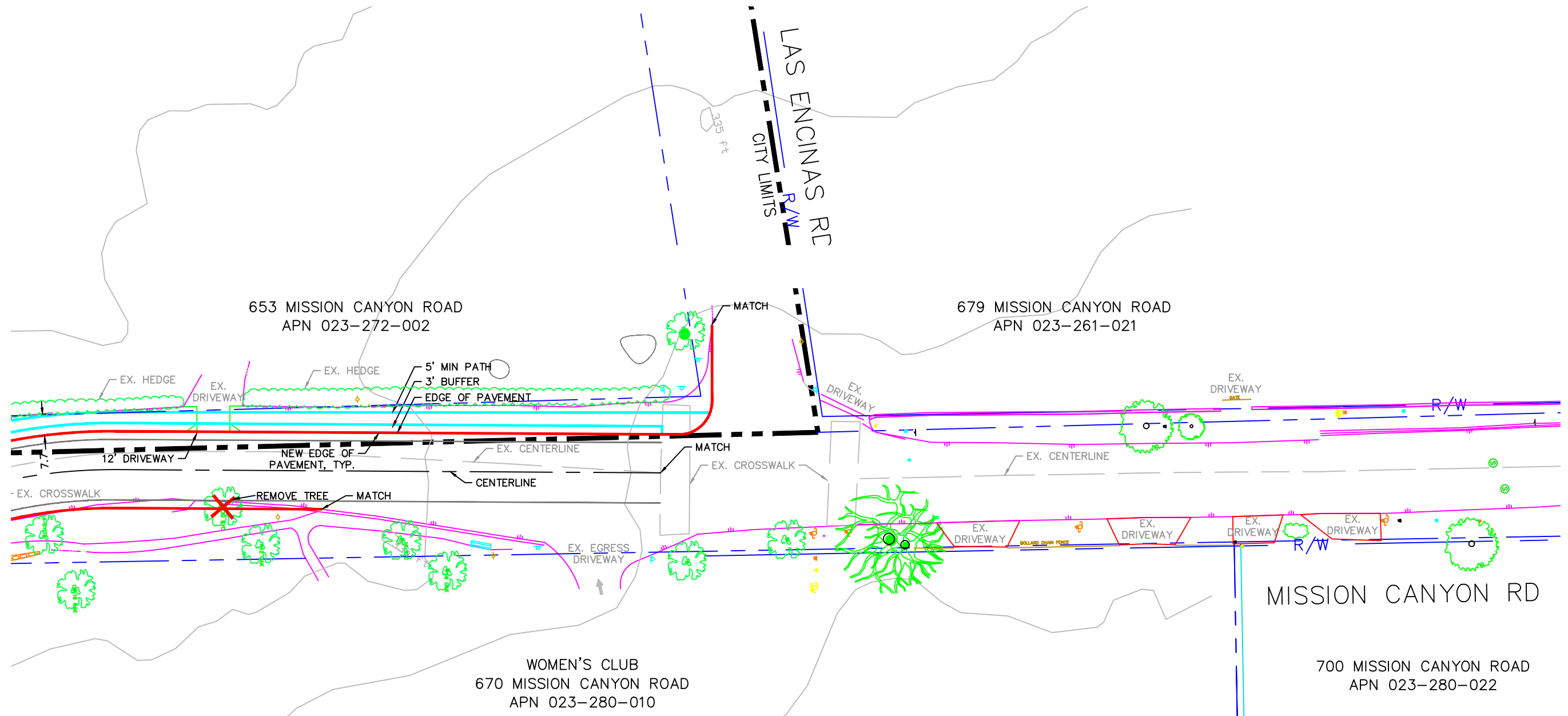
### ALTERNATIVE 1 - LEFT TURN LANE





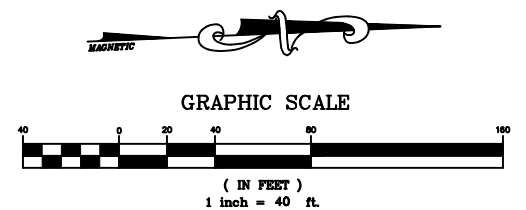
## GENERAL NOTES:

- 1) THIS PLAN IS CONCEPTUAL ONLY. ALL DIMENSIONS SHOWN SHOULD BE FIELD VERIFIED.
- 2) EXISTING IMPROVEMENTS (I.E. DRIVEWAYS, UTILITIES, ETC.) WERE NOT INCLUDED IN THE SCOPE OF THIS CONCEPTUAL DESIGN.
- 3) EXISTING SIGNAGE SHALL BE RELOCATED AS NECESSARY PER THE CITY/COUNTY TRAFFIC ENGINEER.



## MISSION CANYON ROAD AT LAS ENCINAS

### ALTERNATIVE 2 - MATCH STRIPING



I-2.1 – 2.4: Mission Canyon Road at  
Puesta del Sol – Letter to Chris Sneddon  
(July 31, 2014)





# City of Santa Barbara

Public Works Department

[www.SantaBarbaraCA.gov](http://www.SantaBarbaraCA.gov)

August 4, 2014

**Main Office**

630 Garden Street  
P.O. Box 1990  
Santa Barbara, CA  
93102-1990

Mr. Chris Sneddon, Transportation Deputy Director  
Santa Barbara County Public Works  
Transportation Division  
County of Santa Barbara,  
123 East Anapamu Street  
Santa Barbara, CA 93101

**Administration**

Tel: 805.564.5377  
Fax: 805.897.2613

**SUBJECT: Mission Canyon (Corridor) Multimodal Improvement Project –  
Alignment Study at Puesta del Sol**

**Engineering**

Tel: 805.564.5363  
Fax: 805.564.5467

Dear  Mr. Sneddon:

**Facilities**

Tel: 805.564.5415  
Fax: 805.897.2577

As part of the Mission Canyon (Corridor) Multimodal Improvement Project (Project), we've been meeting to discuss the Project, which also includes relocating the crosswalk on Mission Canyon Road at Puesta Del Sol in order encourage pedestrian ease of access between Rocky Nook Park and the Museum of Natural History. To document our discussions, enclosed are drawings of two options to relocate the crosswalk from the north side of the intersection to the south side, in coordination with other pedestrian Project improvements.

**Street Maintenance**

Tel: 805.564.5413  
Fax: 805.897.1991

**Transportation**

Tel: 805.564.5385  
Fax: 805.564.5467

On April 30, 2014, Derrick Bailey, City Supervising Transportation Engineer, and Andrew Grubb, City Project Engineer, met with you and Gary Smart at the intersection of Mission Canyon Road and Puesta Del Sol, regarding the proposed Project. Key discussions of the outcome of the recent Project Community Outreach Process included adding a sidewalk on the west side of Mission Canyon Road between Puesta Del Sol and the Mission Creek Bridge. The proposed sidewalk will create a great opportunity to improve pedestrian access between the County's Rocky Nook Park and the Museum of Natural History, which is located further west down Puesta Del Sol, on the south side of the street.

**Water Resources**

Tel: 805.564.5387  
Fax: 805.897.2613

Taking advantage of the proposed sidewalk by moving the existing crosswalk at the northwest corner of the intersection to the southwest corner will improve sight recognition between vehicles and pedestrians for their safety, and also make it more convenient for pedestrians to go from the Museum to the Park.

As part of the City's conceptual planning design work for the Project, we propose two alternative roadway realignments of Mission Canyon Road at Puesta Del Sol to relocate the existing crosswalk and provide a safe sight distance for pedestrians to cross between the Museum and Rocky Nook Park (see attachments):

- 1) Creating a 4-foot landing near the Women's Club by reducing the existing radius to 184 feet (or 24 miles per hour) and modifying the low rock wall at the entrance to the Women's Club, and

- 2) Matching the existing 198-foot radius and modifying to the low rock wall at the entrance to the Women's Club.

Both alternatives require removal of a 14-foot Sycamore tree and a 30-foot Sycamore tree to create the required sight distance for a 35 mile per hour road. Please note that these exhibits are for conceptual use only and do not include potential impacts to existing improvements (e.g. driveways, utilities, etc.).

The attached drawings are intended to reflect what we agree are two feasible options at this stage of design. We understand the County's final plans will require an ADA-compliant pathway for pedestrians. There still remain several final design issues to resolve, such as final grading, drainage, curb treatments, and appropriate allowances for the offset of immovable features (rocks) off the edge of the roadway, such as the boulders at the entrance of the Woman's Club driveway entrance.

If you have any questions please contact Derrick Bailey, Supervising Transportation Engineer, at 805-564-5544.

Sincerely,



Pat Kelly  
Assistant Public Works Director/City Engineer

PK/sk

Attachment: Mission Canyon Road at Puesta Del Sol Alignment Study (two pages)

cc: Rob Dayton, Principal Transportation Planner  
Derrick Bailey, Supervising Transportation Engineer  
Ms. Rosie Dyste, Senior Planner, Long Range Planning  
Santa Barbara County Planning and Development  
County of Santa Barbara  
123 East Anapamu St., Santa Barbara, CA 93101







