

**Alexander, Jacquelyne**

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**From:** Paulina Conn  
**Sent:** Friday, February 13, 2015 9:24 AM  
**To:** Carbajal, Salud; Wolf, Janet; Farr, Doreen; Adam, Peter; Lavagnino, Steve; sbcob  
**Subject:** Mission Canyon Multimodal Plan

2612 Foothill Rd.  
Santa Barbara, CA 93105

February 13, 2015

Santa Barbara Board of Supervisors  
104 E. Anapamu St.  
Santa Barbara, CA 93101

RE: Mission Park to Mission Canyon Multimodal Improvements Plan

Dear Supervisors Carbajal, Wolf, Farr, Adam, and Lavagnino,

**Please vote Against the Mission Park to Mission Canyon Multimodal Improvements Plan** (“The Plan”). The plan for a pedestrian bridge on the west side of the Mission Creek bridge alters two pristine historic walls, and puts a massive 15 foot hole in another pristine historic wall without increasing safety for pedestrians. It also destroys access to Rocky Nook Park (County) according to City engineers because the path on the east side of E. Los Olivos St. at W. Mountain Dr. (City) would have to be eliminated due to the proposed moving of E. Los Olivos St. eastward. The proposed pedestrian walkway along the west side of Mission Canyon Rd. (County) would continue to be too dangerous even if Mission Canyon Rd. is moved 14 feet eastward taking land away from Rocky Nook park to do this. Please see photo #A of the destroyed wall along which the pedestrian walkway is planned for hundreds of feet right next to traffic at 609 Mission Canyon Rd. (County).

The County must be very vigilant as to what the City's proposals are because, in my opinion, any changes the City decides to make from the Alameda Padre Serra (APS) merge with E. Los Olivos to changes at W. Mountain Drive to the proposed pedestrian bridge on the west side of the Mission Creek bridge with the moving and destroying of historic walls will have a profound effect on the entry to Mission Canyon which is governed by our Mission Canyon Community Plan. City decisions will have an effect on County safety and aesthetics. See and imagine as you look at the photos provided. Photo #B shows the visual entry into Mission Canyon now.

**The City needs to deal with the APS merge with E. Los Olivos before any other capital projects are even considered let alone implemented. We in the community believed this would be part of “The Plan” The City has eliminated this from “The Plan”.**

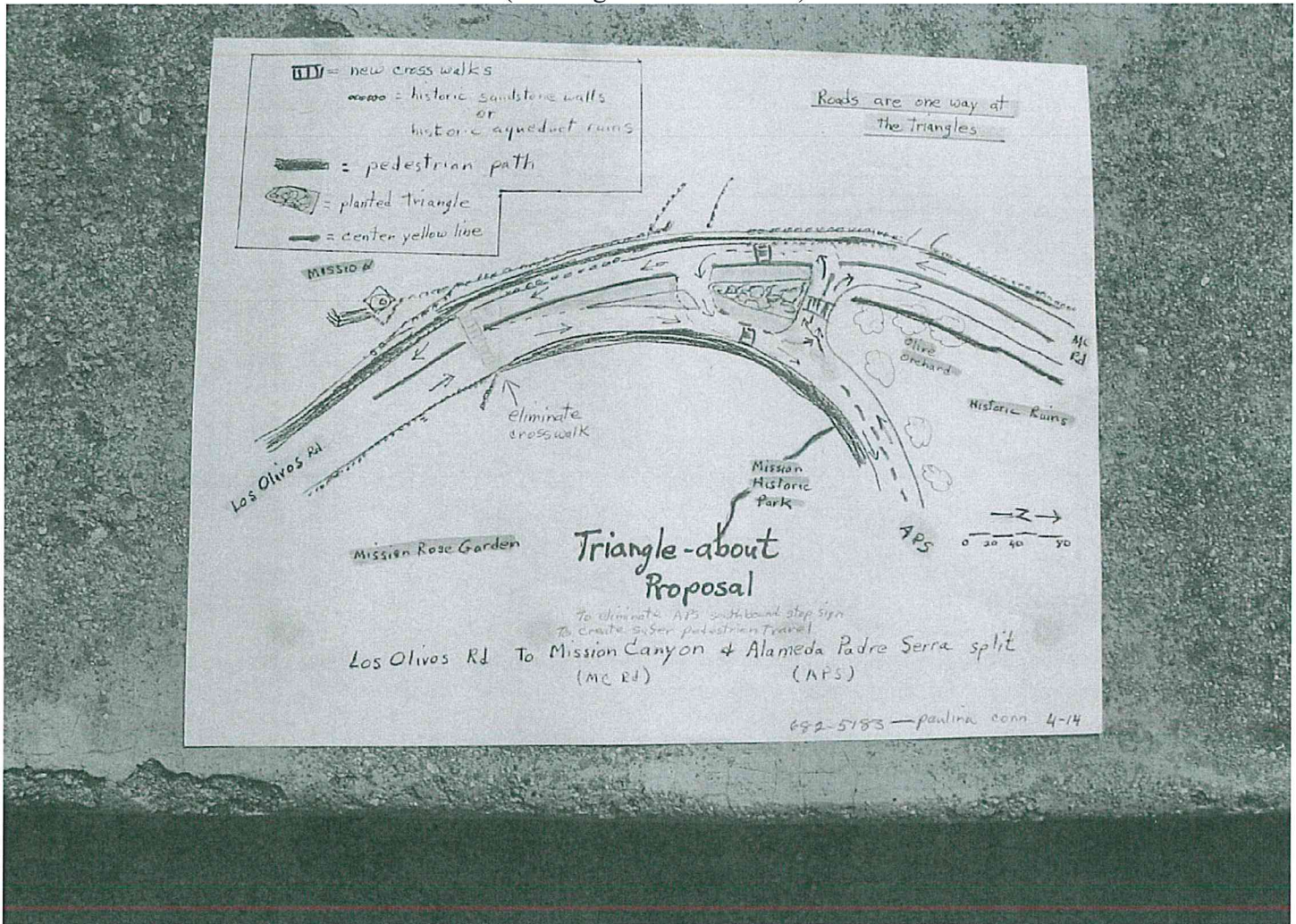
I am providing problems, solutions and photos here under six heading. I have tried to label whether the jurisdiction is City or County. BOLD SYNOPSIS headings are above photos while detailed descriptions are below.

In the first heading are two schematic proposals and photos for the APS/E.Los Olivos area that I think would very likely slow down traffic and solve safety problems for both the City and the County while allowing traffic to flow well.



1. APS/ E/Los Olivos Merge plus a curb cut would allow safer passage for bicyclists and pedestrians and be somewhat sensitive to ADA without destroying our historic heritage or damaging the bucolic entry into Mission canyon. (This is in the City but it effects the County)

IDEA #1. MAKE A TRIANGLE-ABOUT ( a triangular round-about)



IMG\_6590.JPG April, 2014. Schematic of Paulina Conn's proposed APS /E. Los Olivos configuration with a "triangle-about" . The Y at APS and E. Los Olivos becomes one way going north up APS. To go north on E. Los Olivos one has to drive to the top of the triangle, cross to the right hand lane at the top ( or make a single R/L lane) of the triangle and then go north on E. Los Olivos. If going south coming from Mission Canyon Rd and one wants to go north on APS, one has to go to the bottom of the triangle and go north on APS.

IDEA #1. FOR A TRIANGLE-ABOUT THE CURRENT PLANTED TRIANGLE WOULD BE EXPANDED TO INCLUDE THE NORTH BOUND LANE OF E. LOS OLIVOS. AND THE SOUTH BOUND LANE OF APS. CURRENTLY LARGE TRUCKS USE THE BOTTOM OF THE TRIANGLE TO GO FROM E. LOS OLIVOS (SOUTH) TO APS ( NORTH)

IDEA #2. FOR A FREE-FORM T (see schematic below) , THE CURRENT PLANTED TRIANGLE AND THE PAINTED TRIANGLE FARTHER SOUTH WOULD BE ANNEXED TO THE MISSION HISTORIC PARK. APS WOULD BE CLOSED. APS TRAFFIC WOULD ENTER/EXIT E. LOS OLIVOS. ST. AT THE TOP OF THE CURRENT TRIANGLE.

VIEW 1.





IMG\_6462.JPG photo by paulina 5-3-14 1PM. Triangle merge at APS and E. Los Olivos. This could be made into a triangle-about by making E. Los Olivos one way south and APS one way north and the cross-over at the top of the triangle the way to go north or south on E. Los Olivos. A pedestrian crossing can be put in part of the triangle as the triangle will be two lanes wider than it is now. The triangle takes one lane from E. Los Olivos and one lane from APS. The result is a narrower street which is safer for pedestrians to cross especially with the island as a resting place. Bicyclists need to watch vehicle traffic as they go around the triangle.

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IDEA # 2. FOR A FREE-FORM T (see schematic below). THE CURRENT PLANTED TRIANGLE AND THE PAINTED TRIANGLE FARTHER SOUTH WOULD BE ANNEXED TO THE MISSION HISTORIC PARK. APS WOULD BE CLOSED. APS TRAFFIC WOULD ENTER/EXIT E. LOS OLIVOS. ST. AT THE TOP OF THE CURRENT TRIANGLE.

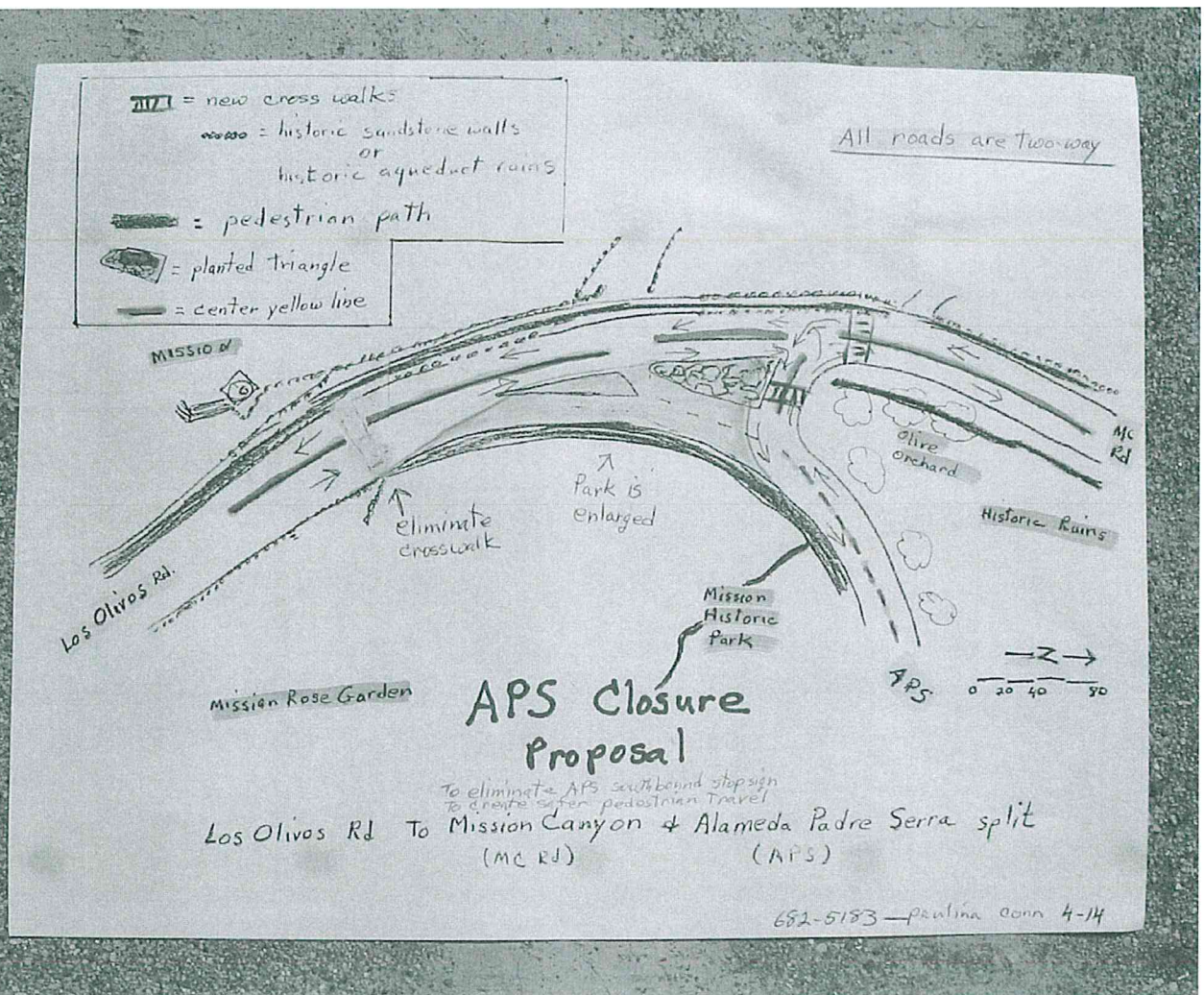
VIEW 2.



IMG\_6460.JPG photo by paulina 5-3-14 1 PM. APS/E. Los Olivos St. merge

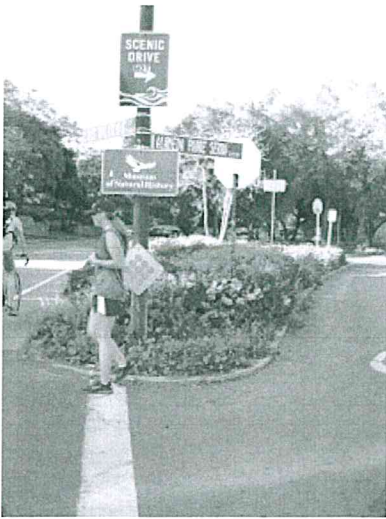
IDEA #2 . MAKE A FREE-FORM T BY CLOSING OFF AT THE BOTTOM OF THE PAINTED TRIANGLE AND INCREASE THE SIZE OF MISSION HISTORIC PARK. THIS ALLOWS FOR SAFER PEDESTRIAN CROSSINGS AND SAFER BICYCLE CROSSINGS. IT WOULD LIKELY SLOW VEHICULAR TRAFFIC DOWN PERHAPS IT WOULD BACK UP. THIS IDEA WOULD NEED TO BE STUDIED. THE NEW PARK AREA COULD BE MADE FLAT ENOUGH FOR DRIVING OVER IN AN EVACUATION EMERGENCY.





IMG\_6591.JPG April, 2014. Schematic of Paulina Conn's proposed APS /E. Los Olivos free-form T configuration . All roads are two way. The Y at APS and E. Los Olivos is closed. The Historic Park is expanded. All road expansions are taken from the top of the triangle not from the historic ruins area. This drawing does not show it, but It is hoped that the intersection will be curved rather than a T to continue the informal curving charctaristic features of the area. Pedestrians would cross at two narrow parts of the road where visibility is excellent for all. The current dangerous, poor visibility crossing at APS to the olive orchard and ruins is eliminated. This configuration is safer bicyclists as they only have one intersection to deal with. Visibility is good and traffic is going slowly or is stopped. The current stop sign at the top of the triangle remains. If traffic gets backed up, the top of the triangle could have three lanes instead of just two to accomodate aput in a left hand turn onto E. Los Olivos. Safety for pedestrians would not be sacrificed as visibility continues to be good. Use the actual photos above to visualize how nice the park would be by closing off the bottom of the APS triangle. The reason for the crosswalk across E. Los Olivos above the triangle is that vehicles are going the slowest here.

BOTH THE TRIANGLE-ABOUT AND THE FREE-FORM T WOULD CREATE GREATER SAFETY FOR PEDESTRIANS. SEE SCHEMATICS ABOVE.



IMG\_7328photo by paulina 2-9-15 5PM. Safer place for a crosswalk at APS and Los E. Los Olivos. In a free-form T everything right of the island would be Historic Park land.  
In a triangle-about this would be part of the triangle where pedestrians could wait to cross a narrower road as the left lane of APS and the right lane of E. Los Olivos would become part of the island.

BICYCLIST IN BIKE PATH NEARING THE TOP OF THE TRIANGLE ON E. LOS OLIVOS.



IMG 7330.photo by paulina 2-9-15, 5 PM. View south from the Historic Ruins towards the Mission. Try to visualize how a triangle-about or a free-form T might work at the top of the triangle.

MAKE A CURB CUT FOR ADA ACCESS TO THE CURRENT PEDESTRIAN BRIDGE. A SMALL PORTION OF THE BICYCLE PATH WOULD BE EFFECTED BY THE CURB CUT LANDING. THIS DOES NOT AFFECT BICYCLE SAFETY AS THERE IS NO CHANGE TO THE BIKE LANE CONFIGURATION. THE DIRT PATH COULD HAVE TO BE STABILIZED AT THIS SMALL POINT.





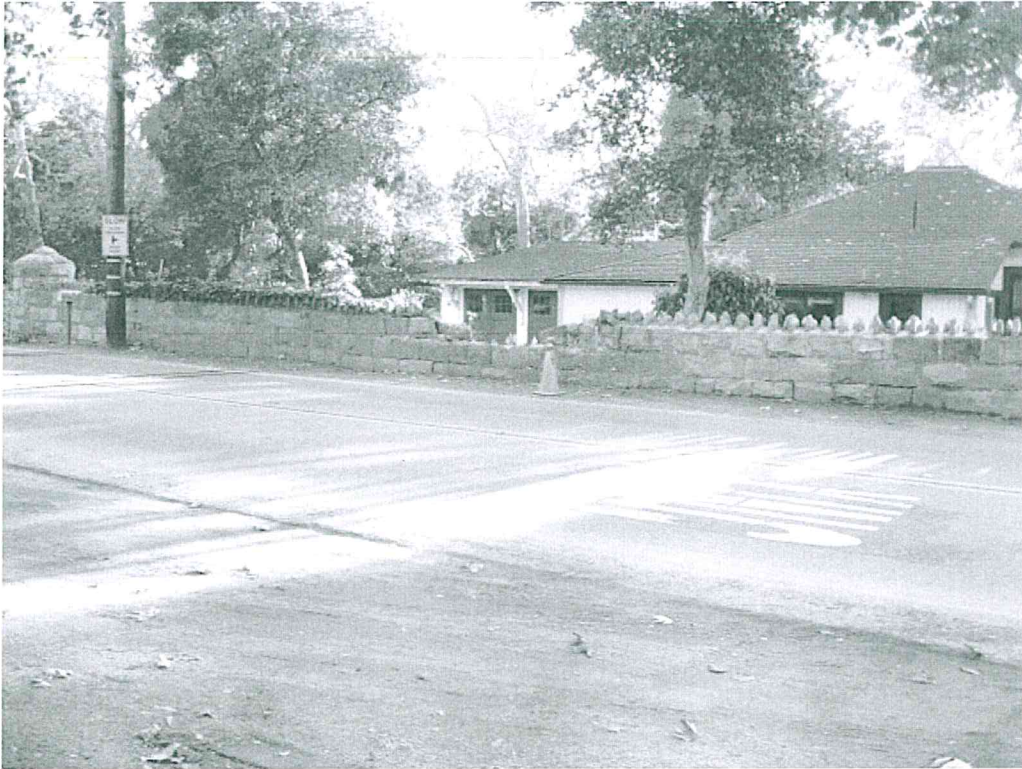
PHOTO #B. IMG\_1087.JPG photo Paulina Conn 11-17-07 Historic Landmark Sand stone bridge over Mission Creek at corner of E. Los Olivos St. and W. Mountain Dr. Looking north. Mountain Dr. is on right. Historic watering trough with inscription is seen on south east corner of bridge. Left is historic sand stone wall of St Mary retreat. On right beyond bridge is Rocky Nook Park. Note the white strip on the road. This is the hatch mark for the bicycle path. Note the historic (1806), still in use, City water reservoir on the right. Where the dirt path ( must stay due to landmark status) dirt is about five feet wide near the white line in the street, the curb could be cut to allow wheel chair and stroller access. The road is flat enough here. Drivers would have to be very vigilant when a wheel chair rider was present. The grade across W. Mountain Dr. is likely more than the 10% ADA allowable but the cross slope is probably not more than the 2% allowable. The pedestrian path across the bridge is in compliance width ( more than 36") and slope (no more than 10%) wise I believe.

**2. Proposed bridge. Do not put in a pedestrian bridge across Mission Creek on the west side until or unless the entire property at 609 Mission Canyon Rd. can be acquired as public property. Then, and only then can there be a safe pedestrian walk on the west side. And then this must be studied for aesthetic and historic compatibility.** The walkway can then go on the west side of the stegosaurus wall with the wall shielding pedestrians from traffic up to Puest del Sol where there is a natural and safe exit at the north west edge of the property. A designated continuation of the path would then need to be created either in front of the Museum Director's property along the road with some sort of barrier or behind the hedge on the Museum Director's property (**needs community discussion**). Then the path continues up Mission Canyon Rd. where it currently exists to Foothill Rd. A new pedestrian bridge should only be put west of the current vehicle bridge if the pedestrian bridge will be unobtrusive and not stick out above the historic landmark Mission Creek bridge. The problem with a pedestrian bridge on the west side will always be the access from the City side as the pinch point is here with the end of the sidewalk hitting a buttress. If pedestrians are willing to be in the bike lane for about four feet then a pedestrian bridge entry/exit on the City side could be created similar to the current one that is on the east side of the Mission Creek bridge. On the County side the exit/entry would only be accessible at Puesta del Sol not immediately at the bridge end as the pathway would go entirely to the west of the stegosaurus wall to Puesta del Sol. Currently the view from the vehicle bridge west is a beautiful natural riparian area. A bridge must not intrude on this. Fish and Game would also have to weigh in on damage the bridge building would have on the endangered trout in the creek here.

PROPOSED DANGEROUS WALKWAY ON THE WEST SIDE OF MISSION CANYON RD. (COUNTY)  
ALL THE WAY FROM THE BRIDGE TO PUESTA DEL SOL A DISTANCE OF ABOUT 200 FEET.  
PART OF THE HISTORIC STEGOSAURUS WALL WOULD BE MOVED WESTWARD. MISSION



CANYON ROAD WOULD BE MOVED EASTWARD. ROCKY NOOK PARK AND THE WOMAN'S CLUB WOULD LOSE PROPERTY. THE ENTRY INTO MISSION CANYON WOULD BE URBANIZED. CURRENT EAST SIDE PEDESTRIAN MISSION CREEK CROSSING WOULD BE INACCESSIBLE ACCORDING TO CITY ENGINEERS. ROCKY NOOK PARK COULD NOT BE ACCESSED FROM THE MISSION ROSE GARDEN OR ROOSEVELT SCHOOL OR ANYOTHER SCHOOL ON THE EAST SIDE.



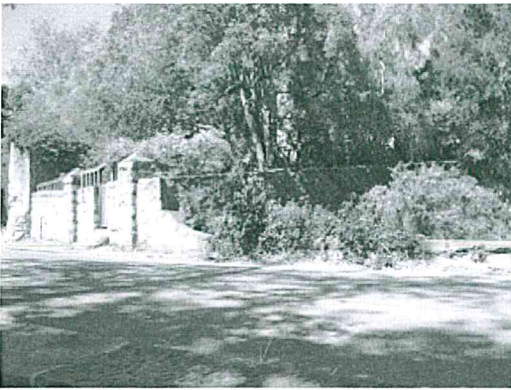
Why the proposed pedestrian bridge on the west side of the Mission Creek vehicle bridge is a bad idea. (County)

PHOTO #A. IMG\_7308 photo by Paulina 2-9-15 4PM. On Saturday, 2-7-15, 609 Mission Canyon Rd. aka the Kay property a 16 foot stretch of the stegosaurus wall in front of which a pedestrian walkway is planned was damaged twice in one day by out of control vehicles (rainy day and night). Note the new cement to the right of the hole. This is another 20 feet or more that was damaged in 2014 also by a speeding vehicle.

Under the proposed bridge plan the road is planned to be moved eastward into the dirt area of Rocky Nook Park (seen here) making a little more room next to the wall but taking away our current meandering sylvan path that is in this area and moving it somewhere and who knows in what sanitized rather than rural configuration or materials. This move will not make pedestrians safer on the west side in my opinion.

PROPOSED WESTSIDE PEDESTRIAN BRIDGE WOULD MOVE HISTORIC WALL WITH GATE WESTWARD. HISTORIC PERPENDICULAR RETAINING WALL WOULD HAVE A HOLE PUT IN FOR BRIDGE PLACEMENT. CURRENTLY THE VIEW OF MISSION CREEK IS A RIPARIAN WOODLAND. UNDER THE PROPOSAL THE PEDESTRIAN BRIDGE WOULD BE VISIBLE FROM THE HISTORIC VEHICLE BRIDGE. THE CURRENT PEDESTRIAN BRIDGE ON THE EAST SIDE IS NOT NOTICABLE TO DRIVERS. THE VIEW IS SERENE AND RURAL. (City)





IMG\_6491.JPG photo by paulina 5-7-14- Wed. 10AM view of gate and "aqueduct wall" and perpendicular creek retaining wall at north east side of Mt. Calvary Monastery property and bridge over Mission Creek as seen from across E. Los Olivos, at W. Mountain Drive (City). The historic hand crafted perpendicular Mission Creek retaining wall is 32" thick. The historic hand crafted aqueduct wall is 18" thick. The historic hand crafted buttresses I did not measure but they are thicker than the aqueduct wall. The wall with the gate would be moved at least 10 feet westward and the retaining wall would have a 15 foot wide hole put in it in order for a pedestrian bridge to be installed. This proposed bridge would then exit in the County at an unsafe location at the roads' edge where a large section of the historic handcrafted stegosaurus wall at 609 Mission Canyon Rd. would be moved westward. The moving of walls distorts historical accuracy creating a lie that has to be explained to children and where generation to come will have to unnecessarily imagine the way it was. These walls have been nominated for Historic Landmark status as an Expansion of City Council Resolution No. 98-008 "Designating Mission Historical Park a City Historic Landmark to Include Stone Walls in and Adjacent to Mission Historical Park and Mission Bridge and Protect Them from Deomolition, Removal, or Relocation". This includes the "stegosaurus" wall along the County's Mission Canyon Rd. The walls meet 11 criteria for Historic Landmark status. Only one criteria is needed.

PROPOSED BRIDGE WOULD MOVE A PART OF THIS HISTORIC STEGOSAURS WALL WESTWARD. PEDESTRIAN EXIT WOULD BE ON THE VERY BUSIEST SECTION OF MISSION CANYON RD. PEDESTRIANS WOULD PROCEED FOR OVER 200 FEET AT THE ROAD'S EDGE JUST SLIGHTLY DIFFERENT FROM WHAT IT IS NOW. SEE PHOTO OF DEMOLISHED WALL ABOVE THAT HAPPENED ON 9-7-15.



IMG\_6488.JPG. photo by paulina 5-7-14 10 AM. View west of bridge (City) over Mission Creek and 609 Mission Canyon Rd., Kay property stegosaurus wall. (County). The proposed pedestrian bridge would exit by moving a section of the stegosaurus wall westward creating an unsafe pedestrian exit onto Mission Canyon Rd. (County) altering the aesthetics of the historic bridge and altering the current natural view over Mission Creek into one of manufactured suburbia with a new pedestrian bridge visible and altering historic reality.

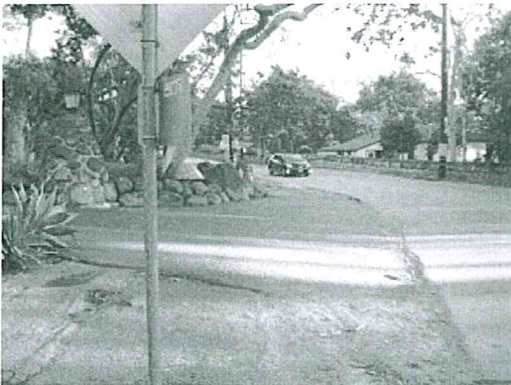
CURRENT UNOBTRUSIVE AND FUNCTIONAL PEDESTRIAN PATH OVER MISSION CREEK. NOTE THE GORGEOUS VIEW ON THE WEST SIDE OF THE ROAD OF THE WOODLAND EDGE OF MISSION CREEK. A BRIDGE WOULD MAKE THIS INTO AN URBAN VIEW INSTEAD OF A RURAL

ONE FROM THIS SPOT AT THE ENTRY TO ROCKY NOOK PARK.



IMG\_6489.JPG. photo by paulina. 4-30-14. 3:50 PM. Mission Creek bridge looking south from County side. This bridge is compliant with American with Disability's Act (ADA) width. Entry here (County side) is 41" to 42" wide. The first northern most slight bend is 42" wide and the next bend is 50" wide. Wooden path varies from 42" to 45" usable width. Entry at south end of this walkway (City side) is 50" wide. Minimum width for ADA compliance is 36". The curves are not too sharp for wheelchair maneuverability.

IF THE WESTSIDE PEDESTRIAN BRIDGE WERE INSTALLED, THE PILLAR AND NATURAL ROCK WORK IN FRONT OF THE WOMAN'S CLUB WOULD BE REMOVED IN ORDER TO MOVE MISSION CANYON RD. EASTWARD. THE MEANDERING PATH HERE WOULD BE LOST. A SYCAMORE TREE WOULD ALSO BE REMOVED. (County)



IMG\_7350 photo by paulina 2-9-15 5PM. As I understand it, with "The Plan" the historic and delightful pillar rock wall in front of the Woman's Club would be destroyed due to the moving eastward of Mission Canyon Rd.

**3. Parking. (County)** I believe that there should be "NO PARKING" anywhere along Mission Canyon Rd. Except for emergency vehicles. "The Plan" does not seem to deal with parking but it effects the safety of the pedestrian pathways. See photos below.

REMOVE NON EMERGENCY VEHICLE PARKING ALL ALONG MISSION CANYON RD. THIS IS ROCKY NOOK PARK ENTRY WITH PEDESTRIAN PATH.





IMG\_7344 photo by paulina 2-9-15 4:45PM . View north at entry to Rocky Nook Park. Note parked vehicle where pedestrians walk. I propose eliminating parking here with a removable rustic fence or plantings that would not impede a fire truck or other emergency vehicle but would deter other vehicles. **A consnsus solution needs community discussion first.**

REMOVE NON EMERGENCY VEHICLE PARKING ALL ALONG MISSION CANYON RD. THIS IS UPPER MISSION CANYON RD ALONG THE GLENDESSARY ESTATE (COUNTY HISTORIC LANDMARK) WALL .



IMG\_7361 photo by paulina 2-12-15 2PM. View south along upper west side of Mission Canyon Rd. Parked cars block safe passage for pedestrians and bicyclists. Plantings and/or removable rustic fencing would deter parking.



**4. Bicyclists Safety. We need a community discussion.** The vehicle bridge over Mission Creek (City) is 34 feet wide according to City traffic engineers and has a bicycle path on either side. I believe that Mission Canyon Rd. (County) is wide enough so a bicycle lane could be painted on either side of Mission Canyon Rd. from the bridge up to Las Encinas Rd. If not then lets have a community discussion as to how to accomplish this if we have the dirt area in front of Rocky Nook Park to “play with”. Foothill Rd. aka State Highway 192, is not a safe route for bicyclists so bicycling up to Foothill should be discouraged. However, some bicyclists will go there anyway. Putting a fog line along the 11' vehicle lane edge from Las Encinas to Foothill will at least define where a bicyclist might ride. A fog line might also slow down traffic as drivers will be more aware of lane width. It might also be considered to allow bicycling on the pedestrian path that is on the west side of Mission Canyon Rd. that goes from Las Encinas to Foothill. The path is about 42” to 48” wide most of the way. The path is not in consistently good condition so repairs need to be made. There are also driveways at steep angles across the path and at Las Encinas there is a telephone pole that blocks easy access. But this path is a safe place for school children and the rest of us to be walking or on bikes.

For bike riders to go to the SB Botanic Garden, I beleive they should go through Rocky Nook Park. After the paved driveway, there is a dirt path to Mission Oaks Lane which is paved. Then it’s easy and relatively safe access to the SBBG.

For bike riders to go to uptown State St. or to Foothill at Alamar, they can make a left on Las Encinas or at Puesta del Sol from Mission Canyon Rd. and decide whether to ride along Puesta del Sol or through the Museum parking lot to get to their destinations in a relatively safe fashion.

**5. Paths for Pedestrian Enjoyment and Safety and ADA Access. There should be a community discussion as to the best way to keep the rural and informal feel for everyone.** Historical Resources do not need to be destroyed to comply with ADA. ADA is a civil rights issue. Historical resources are important for everyone, including the disabled, and are therfore protected and encouraged to be saved for everyone.

The current pedestrian bridge over Mission Creek complies in ADA width and in slope. It looks to me that the spaces between the boards are narrow enough to not present a problem for wheels. There are issues on the City side where a curb cut could be made at the City reservoir and the slope at W. Mountain Drive might be able to be altered.

On the County side of the Mission Creek bridge, the paths will need to be of a well maintained, smooth material and some slopes may need to have bulb outs for wheel chair users to rest. All the paths comply with ADA in width.

I have walked the entire length of Mission Canyon Rd. and measured the current pedestrian pathway, noted the slopes, and taken photos. I believe the pathway can be fixed with little money except perhaps where a phone pole could be moved and at the driveways where the driveway slope is counter to the path direction. The slope could cause wheel chairs to keel over, children on bicycles to have a difficult time, and parents with strollers to have a challenge navigating without going into the street. I believe the rural and rustic nature should and can be kept. The hill by Glendessary Lane is a treasure. I love the path along Rocky Nook Park and the Woman’s Club. These are my favorite parts of the walk.

KEEP ACCESS FROM THE CITY TO PEDESTRIAN BRIDGE ON THE EAST SIDE OF MISSION CANYON RD. IT WORKS.





IMG\_6489.JPG photo by paulina 5-7-14, 10AM. View southwest from Rocky Nook Park entry (County) to pedestrian bridge over Mission Creek. Note the rural vista. A new free standing pedestrian bridge would destroy this.

KEEP MEANDERING PATH IN FRONT OF ROCKY NOOK PARK AND WOMAN'S CLUB. IT IS AN UPLIFTING EXPERIENCE TO WALK IT. (County)



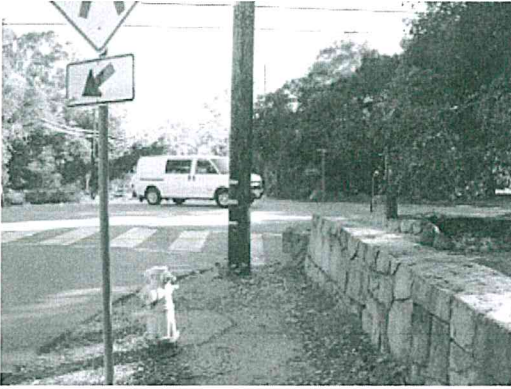
IMG\_6439.JPG photo by paulina 5-3-14, 12:30 PM. Path and road at women's club view south. (County)

CREATE A PATH WHERE IT IS MISSING IN FRONT OF THEMUSEUM OF NATURAL HISOTRY DIRECTOR'S HOUSE BETWEEN PUESTA DEL SOL AND LAS ENCINAS.



IMG\_7295photo by paulina 2-9-15-5PM. View south from west side of MC Rd. between Las Encinas and Puesta del Sol. A pedestrian path needs to be defined here. The 20 MPH curve sign is hidden for pedestrians and motorists so the hedge needs pruning for safety purposes. A path needs to be defined here either by a shoulder of a different substance or put behind the hedge which the property of theMuseum of Natural History ( Directors's home). In eather cas the road width needs to be narrowed to the 11 feet that it is supposed to be. This will likely slow down the vehicular speed that seems to be causing crashes into the stetosaurus wall south of the Puesta del Sol intersection.

MOVE PHONE POLE FOR ADA ACCESS.



IMG\_7290 photo by paulina 2-9-15, 4:15 PM. View looking south. Phone pole is in the way for wheel chair access on the north west corner of Mission Canyon Rd, and Las Encinas. It is awkward for pedestrians and kids on bikes too. The north side of the intersection is the safer crosswalk to use across Mission Canyon Rd. as the road is narrower and a person on foot need not worry about the left hand turn traffic or the traffic going straight maneuvering around the vehicle on the right side.

FIX WALLS SO THEY DO NOT IMPINGE ON PATHS.



IMG\_6436.JPG photo by paulina 5-3-14, 12:30 PM. 789 MC Rd. Wall is easily repaired. Rocks are not cemented.

KEEP HILL AND OAKS NORTH OF GLENDESSARY LANE.



IMG\_6383.JPG photo by paulina 4-30-14. 2:40 P.M. Wed. Pedestrian path goes up and down a hill along the wall of the Glendessary estate. The path itself is 54" wide at the top of the hill. This area is wide enough for an ADA rest stop.

**5. Crosswalks. We might need a short community discussion.** "The Plan" suggests a crosswalk on Mission Canyon Rd. at the south intersection with Puesta del Sol. Visability is pretty good but vehicle traffic can be moving fast. The north side of the intersection would be better in terms of traffic being able to slow down in a timely manner but visability is impeded due to a phone pole and a sign when a pedestrian goe from the east side of the road to the west. Visability is fine from west to east on the north side of the intersection. I found it more difficult to even think about crossing Mission Canyon from west to east from the south side of Puesta del Sol because of having to cross the fast moving traffic coming north on Mission Canyon Rd which for the pedestrian is the far side of the street.



"The Plan" also suggests a crosswalk across Mission Canyon Rd. from the south side of the intersection with Las Encinas. This is the most dangerous place because of traffic turning left and other passing the vehicle on the right. Mission Canyon Rd. is wider on the south than on the north side. I recommend that the pedestrian crossing be on the north side because the road is narrower and pedestrians do not have to deal with the left hand turn lane. Visibility is good.

PLACE FOR PROPOSED NEW CROSSWALK AT PUESTA DEL SOL AND MISSION CANYON RD. VISIBILITY IS NOT GREAT FROM WEST TO EAST (this photo) BUT IS BETTER FROM EAST TO WEST.



IMG\_7303 photo by paulina 2-9-15, 4:30 PM looking east from south corner of Puesta Sol to across Mission Canyon Rd. to Woman's Club. Trolley is turning right out of the one way Puesta del Sol where "The Plan" proposes a crosswalk. Traffic may also turn left here.

CURRENT CROSSWALK IS ON THE NORTH SIDE OF PUESTA DEL SOL TO CROSS MISSION CANYON RD. VISIBILITY IS GOOD GOING FROM WEST TO EAST. VISIBILITY IS BLOCKED GOING EAST TO WEST BY A PHONE POLE AND A GUY WIRE. NOTE THAT 20MPH CURVE SIGN IS HIDDEN BY HEDGE. THIS MAY BE PART OF THE REASON THERE HAVE BEEN ACCIDENTS WITH VEHICLES CAREENING INTO THE STEGOSAURUS WALL AT 609 MISSION CANYON RD.



IMG\_7297 - current crosswalk that crosses MC Rd. at the north west corner of Puesta del Sol (one way street going east) is at the yellow pedestrian crossing sign. Note that on the east side of MC Rd there is a phone pole and a guy wire that block visibility for crossing to the west side of MC Rd. Crossing MC Rd. from the west side works nicely here. Note the hidden 20MPH curve sign. The hedge should be trimmed so this critical sign is not hidden. Drivers do not slow down in time to avoid hitting the stegosaurus wall in front of the Kay property. A new and larger 20MPH CURVE sign should also be put at the yellow arrow just south of Puesta del Sol. It would be visible from a distance in case the first one is missed.

"THE PLAN" PROPOSED CROSSWALK IS ON THE SOUTH SIDE OF THE LAS ENCINAS INTERSECTION. THIS IS A LESS SAFE IDEA THAN CROSSING AT THE NORTH SIDE OF THE INTERSECTION. (this photos below). CARS GOING STRAIGHT USUALLY SIDE RIGHT AROUND THE VEHICLES TURNING LEFT. THERE IS NO OFFICIAL LEFT TURN LANE HERE. IT IS SAFER FOR PEDESTRIANS TO CROSS MISSION CANYON. RD. AT THE NORTH SIDE OF THE INTERSECTION.



IMG\_6425.JPGphoto by paulina 4-30-14. 4 PM. Mission Canyon Rd. looking north from in front of the bus stop and Women's Club. Bike turning left using proper vehicle lane from MC Rd north onto Las Encinas. She signaled a left had turn as well. This south side of the Las Encinas intersection is wider than the north side and therefore less safe for a pedestrian crossing.

INTERSECTION OF MISSION CANYON RD. AND LAS ENCINAS. NOTE CAR TRYING TO PASS SUV MAKING A LEFT HAND TURN. BUS BENCH IS ON THE FAR RIGHT.



IMG\_6424.JPGphoto by paulina 4-30-14. 4 PM. Mission Canyon Rd. looking north from near the bus stop in front of the Women's Club. There is a vehicle turning left onto Las Encinas Rd. while another vehicle slides around on the right. There is also a vehicle going south in front of Las Encinas. It is safer for pedestrians to cross on the north side of the intersection because the road is narrower and the left hand turn lane is avoided. A fog line marking the road's edge would make this area safer too.

CROSS WALK ON MISSION CANYON RD. ON THE NORTH SIDE OF THE LAS ENCINAS INTERSECTION. THIS IS THE SAFE PLACE TO CROSS THE ROAD. THE ROAD IS NARROW. VISIBILITY IS GOOD. THERE ARE FEWER VEHICLES TO DEAL WITH.





IMG\_7294 photo paulina 2-9-15, 4PM. Pedestrian crossing at Mission Canyon Rd at Las Encinas, north side of intersection. Road is narrower here than on the south side of the intersection. It feels much safer because of the narrow road and no vehicles making left hand turns with others passing on the right side.

## **6. Safety fixes now for little cost along E. Los Olivos / Mission Canyon Rd. from Laguna to Las Encina. (City and County)**

Slow traffic down in all ways possible from the Mission(City ) to Foothill Rd. (County). Blind driveways need to be considered in the speed limit.

Create 25MPH road situation from Laguna at E. Los Olivos (City) in front of the Mission to Las Encinas at Mission Canyon Rd. (County)

Put stop sign on south bound Mission Canyon Rd at intersection with Las Encinas (County),

Put road hump across Mission Canyon Rd. in front of Museum Director's property (County).

Mustn't impede rain water flow.

Put road hump across E. Los Olivos in front of the Mission (City). Mustn't impinge on bicycle lane.

Maintain the white striping for bike lanes and fog lines so they are visible at all times (City)

Create a new bike lane on both sides of the road between the Mission Bridge and Las Encinas Rd. on Mission Canyon Rd. (County)

Remove the wisteria off the stegosaurus wall (County/City) and put it inside the wall to improve visibility for all.

Keep vegetation off the Mission Bridge walls (City) to improve visibility all the time.

Prune hedge that hides the 20MPH CURVE sign in front of the Museum Director's property (County).

See second photo under

"Crosswalks".

Add a new, huge, 20MPH CURVE sign at the south side of the intersection of Mission Canyon Rd. and Puesta del Sol where the yellow

arrow is. See second photo under "Crosswalks" (County). This will be visible for a far distance and may prevent the accidents at the stegosaurus wall.

Narrow the road both visually and actually with plantings, white road edge lines at the 10 foot mark in the City and the 11 foot mark in the

County. **May need public discussion in the County for acceptable solution.**

### **CONCLUSION:**

**Please vote AGAINST this Multimodal Plan. It destroys too much of our collective valued historic and aesthetic heritage without improving safety because:**

The road widening will destroy the finally successful fix to the drainage problem that was done a number of years ago between Rocky Nook Park and the Woman's Club. This area used to flood in the rain.

The flawed pedestrian bridge/pathplan that exits onto Mission Canyon Rd. will endanger too many pedestrians for too long a stretch of very dangerous road along the stegosaurus

wall. See large photo under "2. Proposed Bridge" above. Now the path is safe and the crosswalks are working well.

The City should fix the APS/ E Los Olivos intersections first. This will provide the greatest safety for all. Most of the accidents occur here.

A traffic route with curves, narrow areas, barriers, stop signs, vegetation, and other calming devices are the safest because drivers pay attention. In the case of Mission Canyon this style is part of our heritage and what the community wants saved.

The County does not need "The Plan" as most of the fixes in the County are inexpensive. The continuous path is the only part that will take some capital improvement money or perhaps just maintenance money as the path is already there.

Thank you so very much for your consideration.

Sincerely,  
Paulina Conn  
682-5183

43 year resident of Mission Canyon who has walked, driven, biked, and explored much of the area and continues to do so.